



MONEY HILL

05.05.15

ASHBY-DE-LA-ZOUCH

DESIGN AND ACCESS STATEMENT

The Money Hill Consortium



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1. INTRODUCTION

1.1 The Application

This Design and Access Statement has been prepared on behalf of The Money Hill Consortium (MHC). The application site is 42.03 hectares in area and is identified as part of a 'broad location' for housing-led development. This sits within an assessment area used for technical appraisals (66.16Ha). The proposals described in this document (hereafter referred to as Money Hill) includes the delivery of 605 new dwellings, a new primary school, a potential health centre, a potential community centre, other local centre uses, a new public open space, a new link road to the A511 bypass and a new access point at Nottingham Road.

1.2 Vision

The development of Money Hill represents a unique opportunity for the growth of Ashby-de-la-Zouch (hereafter referred to as Ashby). It will not only enable the town to preserve its town centre character, but it will also provide for the community needs of the future. A sensitive approach to the masterplanning of the site will:-

- Provide additional urban and landscape areas that will contribute to the regional identity of the town;
- Be respectful of the existing townscape of Ashby and its landscape setting;
- Contribute towards the character of the town by introducing a new neighbourhood built for 21st century living; and
- Benefit existing and new residents with a balance of new homes and social amenities, including a range of new sustainability measures.

The following themes elaborate on this vision:-

'Bringing' the National Forest into Ashby

In line with guidance published by the National Forest Company, over 30% of the proposed Money Hill Masterplan is comprised of new open space and woodland that will be within walking distance of the town centre.

New homes within a woodland setting

A distinctive woodland and meadow character will be created through the Money Hill Masterplan, which places buildings sensitively within a landscape setting. Key existing landscape features will be preserved and protected in order to create a unique sense of place for the new community.

Contributing to the town's vitality

The proposals for Money Hill include the potential provision of a number of new community facilities within walking distance of the existing town centre. They also include new pedestrian and cycle links to connect the development into the existing movement network around the site.

Buildings that have green credentials

The new buildings at Money Hill will be carefully located to maximise views of open spaces and will be oriented to benefit from good solar gain. New homes will incorporate renewable energy and water efficient systems.

A distinctive woodland and meadow character

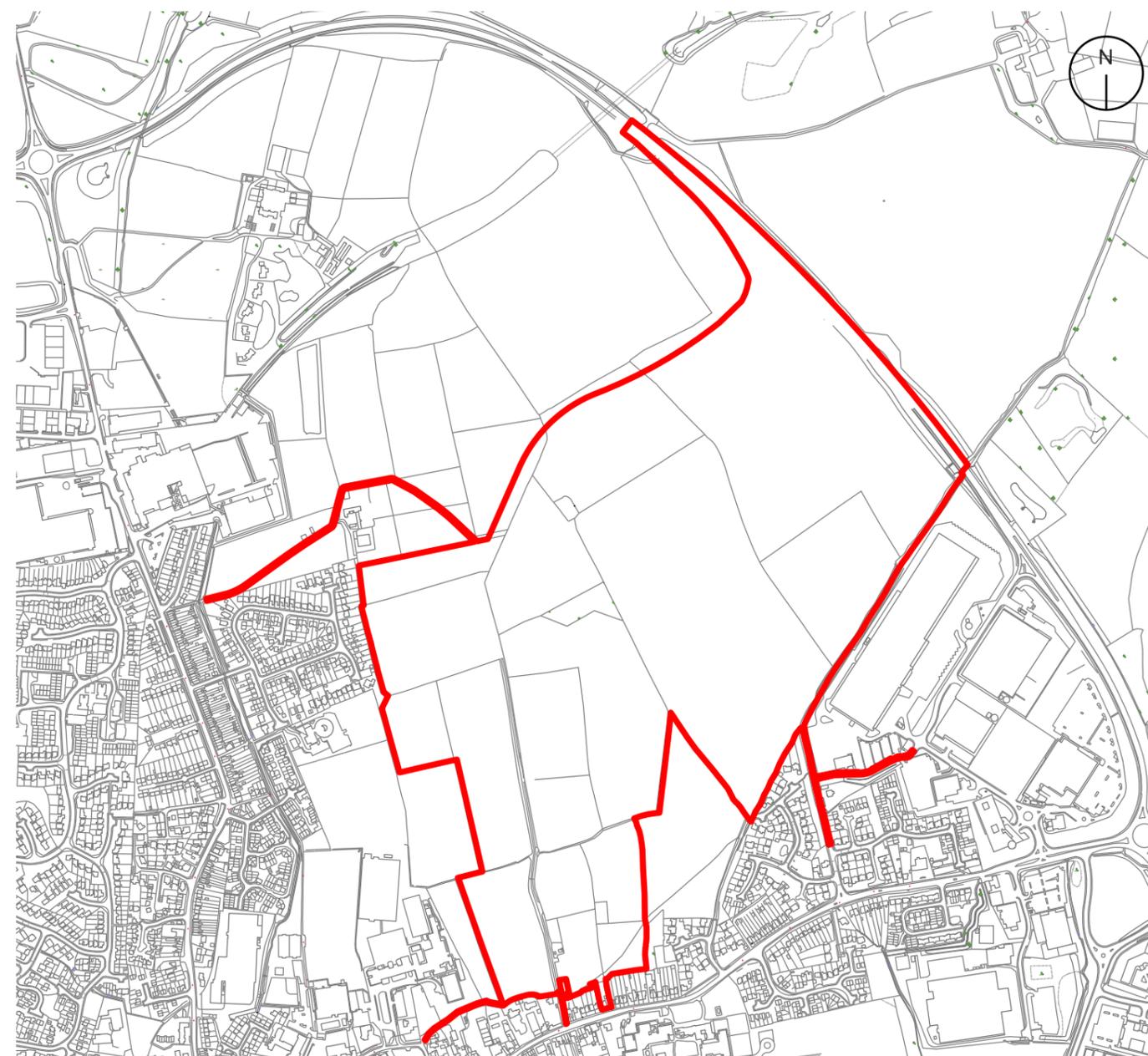
The Money Hill Masterplan includes a number of character areas, the inspiration behind which is the existing natural and historic landscape. The building materials used will reflect the existing local architectural character of Ashby and the site's woodland setting.

Improved local wildlife habitats and biodiversity

Biodiversity across the site will be improved through the creation of new ecological habitats and wildlife corridors within the landscape structure. The proposed landscape structure is arranged around a green grid of hedgerows that connects areas of woodland together.

Protecting the town from flood events

A comprehensive sustainable drainage system (SuDS), designed to attenuate water run off on site without directly discharging into town's watercourses, will be implemented at Money Hill.



Assessment Boundary Plan

Assessment boundary

1.3 The Structure of This Document

This Design and Access Statement provides details on the proposals for Money Hill in terms of Use, Amount, Layout, Scale, Landscape and Character. It explains the ways in which the applicant has developed the design proposals and demonstrates a detailed understanding of what is appropriate and feasible for the site in its context. It also clearly explains and justifies the design and access principles that will be used to develop future details of the scheme. As such, it forms a link between the outline permission currently sought and the future consideration of Reserved Matters applications. This is in line with Guidance on information requirements and validation (CLG March 2010), which states that Local Planning Authorities should ensure that any development given outline planning permission will be constrained by the parameters described in the Design and Access Statement submitted in support of the outline application and that any future decisions relating to that outline permission are consistent with the submitted statement.

The design and access principles set out in this document have been informed by thorough analysis of the site and its context, relevant consultations and adherence to good practice urban design principles and planning policy.

This document is compliant with published guidance from CABI (Commission for Architecture and the Built Environment) as found in Design and Access Statements: How to write, read and use them (CABI 2006), and Guidance on information requirements and validation (CLG March 2010).

“A design and access statement is a short report accompanying and supporting a planning application to illustrate the process that has led to the development proposal, and to explain the proposal in a structured way”. Guidance on information requirements and validation (CLG March 2010).

This Design and Access Statement is presented in the following seven sections.

Section 1 (this section) introduces the document and sets up the broad structure that it will follow.

Section 2 examines the context of the application site, broadly following the guidance set out by CLG in Design and Access Statements: How to write, read and use them (CABI 2006). This section concludes with a concise evaluation of the physical constraints presented by the site.

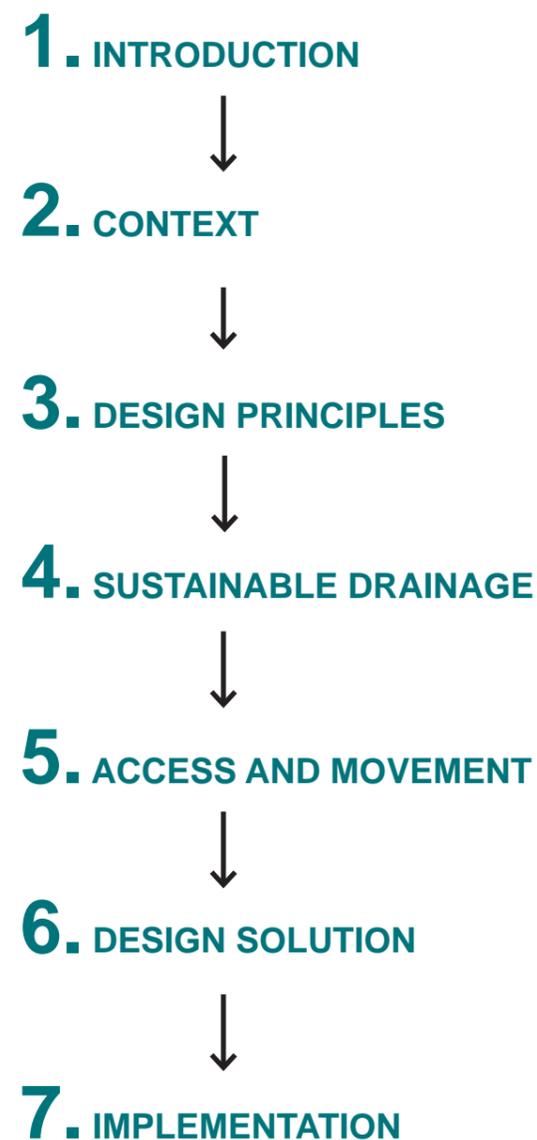
Section 3 outlines the broad principles behind the proposed design, generally following the parameters set out in the CLG Guidance. These principles are primarily informed by the site analysis presented in Section 2.

Section 4 presents the sustainable drainage framework for the site and outlines the principles and components of the proposed drainage strategy.

Section 5 presents the access and movement parameters for the site and outlines the inclusive access principles that have informed the proposals.

Section 6 consolidates the analysis described in each of the previous sections by presenting the design solution in the form of character areas, an illustrative Masterplan and parameter plans.

Section 7 sets out proposals for the implementation of the Masterplan, including Building for Life 12 assessment and phasing.



Document Structure Diagram

2. CONTEXT

2.1.1 Ashby-de-la-Zouch

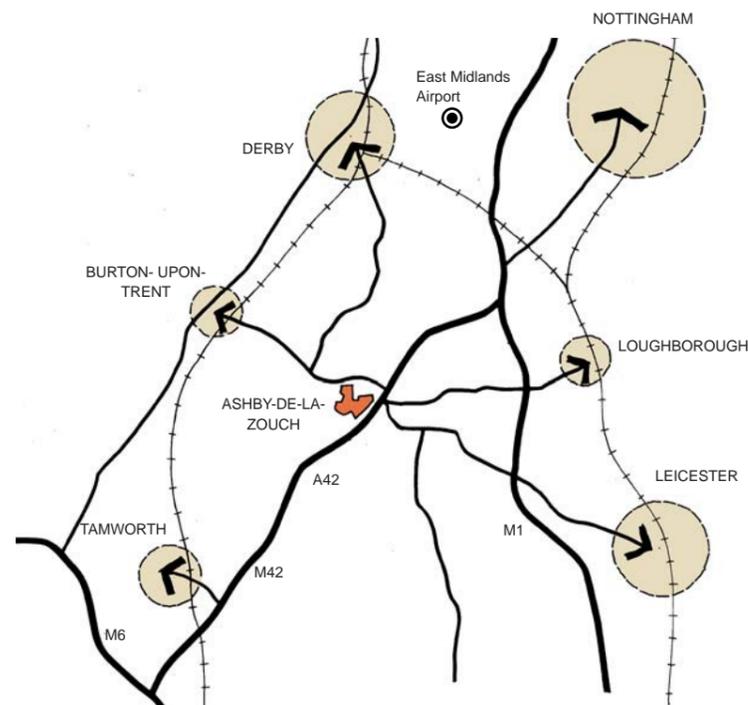
The application site benefits from good links to a number of larger towns and employment centres, all within 15 miles of the site by road. Coalville and Leicester are located to the south-east of Ashby, and are accessible by the A511. Loughborough, also to the east is linked to Ashby by the A215. Swadlincote and Burton-upon-Trent are located to the west and are linked to the town by the A511. East Midlands International Airport is a significant source of employment in the locality. It is found to the north-east of Ashby at Castle Donnington and can be accessed via the A42.

In addition to the road connections, Ashby is also close to major north-south rail commuter lines including the West Coast Main Line and Cross Country Line, offering connections to the west, the London to Leeds Line from Tamworth and Burton-Upon -Trent and the London to Sheffield Midland Main Line which passes through Leicester, Loughborough and Nottingham.

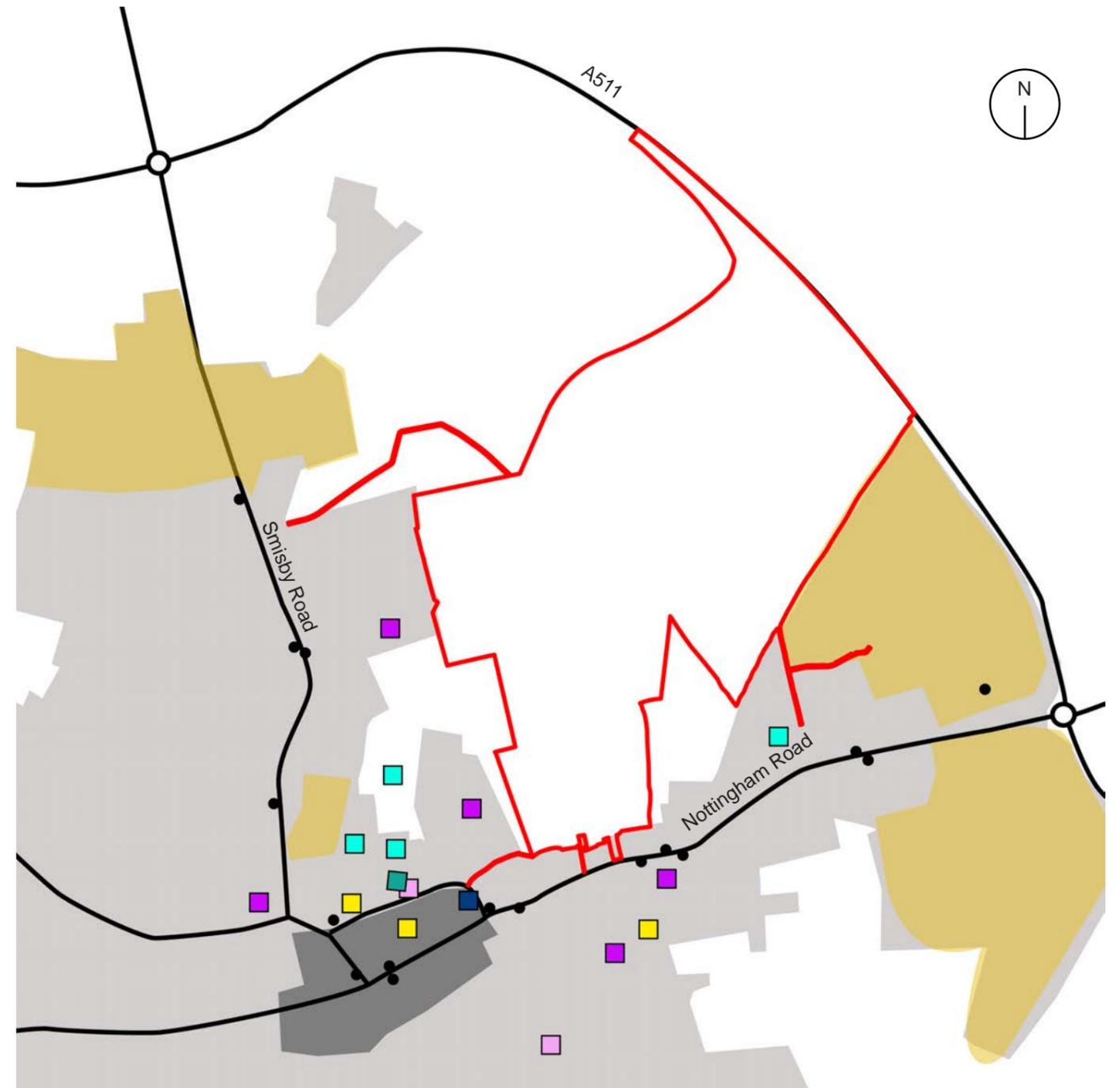
2.1.2 Local Facilities

The application site is centrally located in Ashby, North West Leicestershire. It is adjacent to the designated Local Centre, where a wide range of shops, restaurants, services and facilities are located. Ashby School, Ashby District Hospital and Ivanhoe Specialist Technology College are all located within 0.2 miles of the site.

To the east is a large employment area, which includes the main UK distribution centre of United Biscuits, an important local employer. To the north-east the A511 bypasses the town centre providing a link around Ashby and connecting into the wider strategic road network. To the west is Ivanhoe College, Hood Park, Woodcote Primary School and properties along Money Hill and Allison Close.



Strategic Location Plan



Existing Local Facilities Plan

2.2.1 Ashby's Urban Growth

This series of plans illustrates how Ashby has changed over the past 130 years. It shows the impacts of rail, industry and, more recently, road infrastructure and how these have contributed to the urban character of the town.



1886
 A typical small market town with a market street and an agricultural hinterland.



1925
 Slow incremental growth of housing and industry closely associated with rail and road infrastructure, particularly in the Nottingham Road area.



1955
 Continued incremental growth of housing and industry closely associated with rail and road infrastructure, particularly in the Smisby Road area.



1980
 The railway is decommissioned and car ownership increases, resulting in housing and industrial infill development that is reliant on the distributor road network.



1993
 Growth of infill development, urban expansion to the west of the town centre and the establishment of the A45 to the east all contribute to circulation pressure within the town centre.



2012
 Industrial growth associated with the A45 continues. the A511 by-pass is established to relieve circulation pressure within the town centre, and as a result out-of-town development increases.

2.2.2 Ashby Town Centre Characteristics

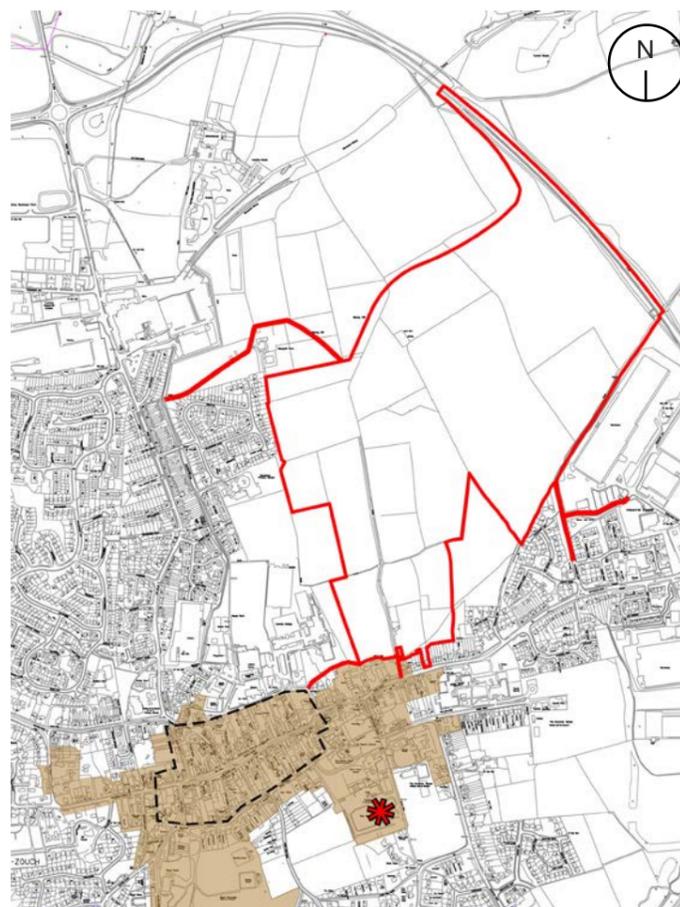
As a result of its adjacency to the application site, Ashby's historic town centre is an important consideration in the development of Money Hill. Although the town centre is close to the location of Ashby's early medieval core, evidence of which is still visible in its rectilinear urban grain, the predominant visual character of the town centre today is Georgian and Victorian. The main commercial street in the town centre is 'Market Street', a complete historic high street featuring a rich mix of architectural styles and uses.

Ashby Conservation Area extends to the south-western boundary of the Money Hill site. There are a number of listed buildings within 500m of the southern boundary, including the Grade I Listed Ashby Castle. Closer to the site are four Grade II Listed Buildings on the northern side of Wood Street. However, recent development to the rear of these buildings, immediately adjacent to the site boundary, has already compromised their settings.

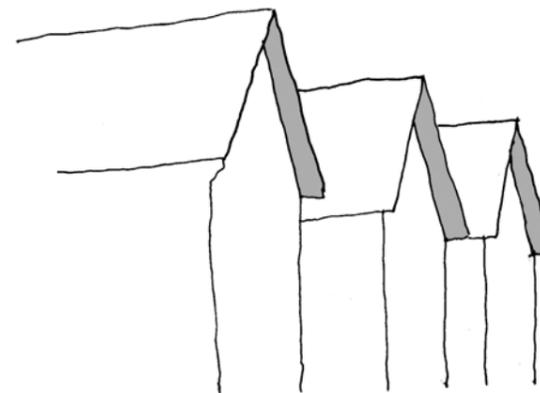
Beyond the town centre, a number of significant domestic architectural features can be identified as characteristic of Ashby. These include:-

- Gables, expressed as deep eaves, pronounced barge boards or ridge details;
- Chimneys, mostly set on ridges away from gables or mounted within the roof slope;
- Symmetrical semi-detached villas, with elevational compositions made up of bay windows and gables;
- Houses on slopes with a range of set-back distances;
- Archways through buildings that provide rear service access; and
- Red brick, white half render and slate roofing.

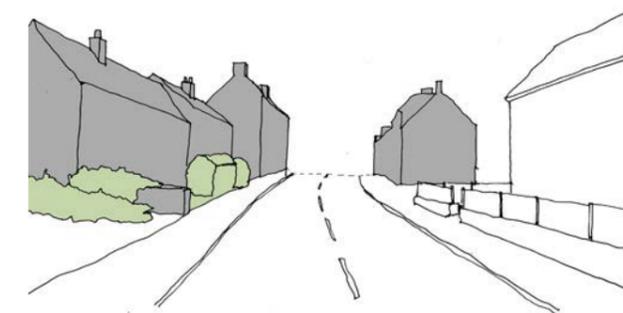
These are not to be replicated within Money Hill, but will inform the architectural expression of the new development.



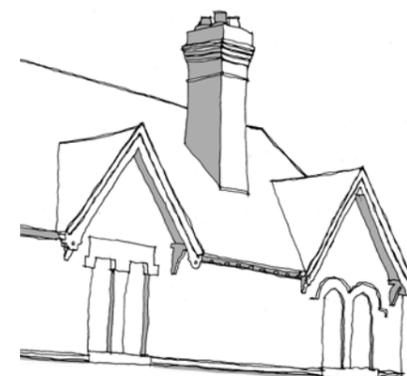
Plan showing the heritage constraints on the site, including the north-eastern portion of Ashby Conservation Area which is adjacent to the site



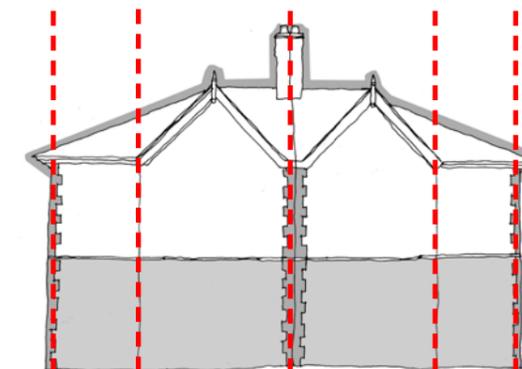
Gables fronting the street and 45 degree pitched roofs



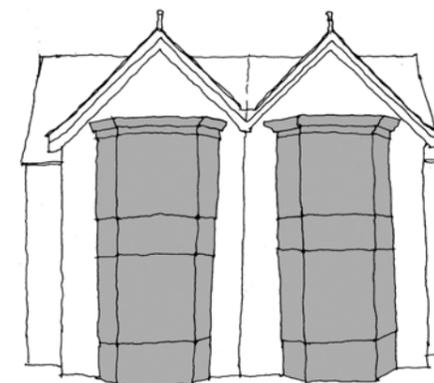
Buildings, arranged to step up the slope enclose the street with varying set backs and roof lines



Chimney stacks and roof gable details above windows



Semi detached houses with good symmetry, brick detailing and half render



Statement bay double height windows with gable detailing



Archways through buildings.

Sketches illustrating the key architectural characteristics of Ashby

2.2.3 The Neighbourhoods Surrounding Money Hill

The site is defined by a variety of boundary conditions, including the A511, agricultural fields, industrial warehousing, residential development and school playing fields. To the west and south there are 5 residential areas that adjoin the site boundary. Although almost exclusively residential, the buildings on these streets exhibit a number of different architectural styles and date from different periods in time.

The key positive and negative features of these residential areas are listed below.

Wood Street

- Historic terraced houses;
- 2 - 3 storeys in height;
- Narrow or no set backs from the street;
- Red brick, white render and slate roofs; and
- Roofs have dormer windows and large chimneys.

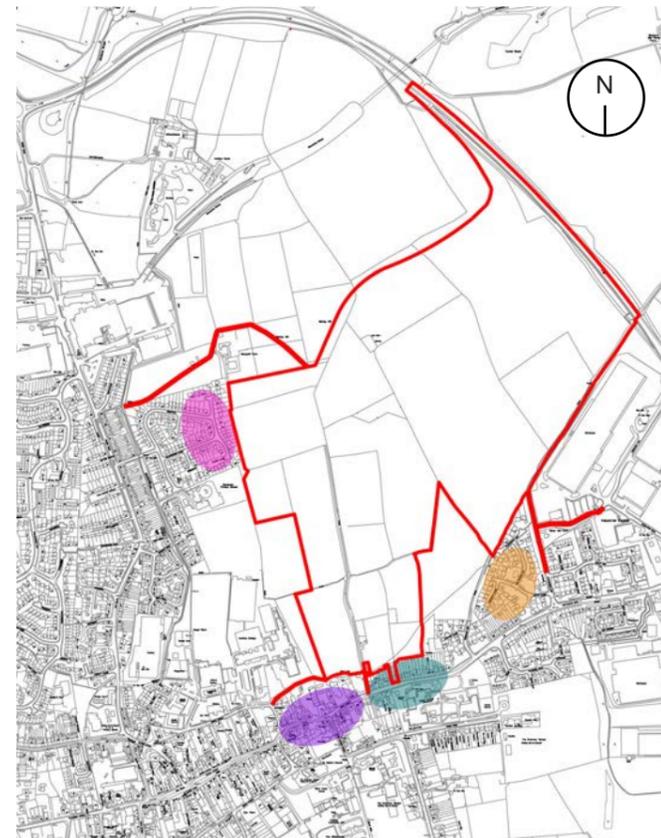
Nottingham Road

- Victorian / Edwardian detached and semi-detached houses;
- Up to 6m set backs from the street;
- Front gardens, defined by hedges or brick walls, that are large enough to have mature trees in them;
- Red brick, white render and slate or red tile roofs;
- Brick or timber detailing;
- Many large chimneys; and
- Gables oriented to face the street, in many cases intersecting the houses' roofs, creating a well-expressed rhythm along the street.

Money Hill and Allison Close

- 1970's, 2 storey and bungalow development;
- Mostly detached houses with medium sized set-backs;
- Pale coloured brick, white window frames and grey roof tiles; and
- Generally poor street scene, with a lack of boundary continuity and tree planting.

- Wood Street (positive sense of place)
- Nottingham Road (positive sense of place)
- Fairfax Close (poor sense of place)
- Money Hill / Allison Close (poor sense of place)



Plan showing the neighbourhoods surrounding the site



Typical Nottingham Road detached white render and brick property set back from street



Semi detached 2.5 storey Victorian property with hedge boundary treatment on Nottingham Road



Detached gable end properties along Money Hill and Allison Close that are set back from the street with low boundary walls



Typical large detached villa style properties near Fairfax Close and Woodcock Way with large set-backs from street

2.3.1 The Character of the Site

The assessment area itself extends over 69 ha of arable and low quality grass pasture. There are a number of hedgerows, mostly to the south and west of the site, that divide the pasture into a small-scale pattern of bounded fields. The nature of this field pattern hints at the long agricultural history of the land. Most of the hedgerows run north to south and many are significantly overgrown.

To the north-east there are larger arable fields that are divided by thinner hedgerows with occasional hedgerow trees.

The historic Ivanhoe Way, designated as part of the National Trail, passes through the southern section of the site. To the south of the Ivanhoe Way, and running alongside part of it, there are large areas of unmanaged long grass and scrubland. In the fields adjacent to the Ivanhoe Way there is an area of 'ridge and furrow', indicated by the archaeological pattern of ridges that were left by the historic ploughing of the land.

The highest point (162m AOD) of the site is to the north-east close to the A511. The lowest point (129m AOD) of the site is to the south-west adjacent to Ivanhoe Specialist Technology College sports fields.

Other notable features include a watercourse and spring found towards the centre of the site and flowing through its southern sections. There is also an existing copse of mature trees located in the centre of the site. To the west there is a farm track, sections of which are delineated by a double hedgerow.

In general, the site suffers from poor drainage as a result of the clay subsoil.

Most of the trees on site are of poor quality, with some tree groupings and individual trees that are significant. Some of the hedgerows are archaeologically and ecologically important and will be retained and enhanced.



Looking east from the Ivanhoe Way footpath towards the United Biscuit factory and existing properties on Fairfax Close



Typical existing rear garden boundary treatment along the southern boundary of the site



Looking north from Ivanhoe Way the landform slopes down towards the watercourse



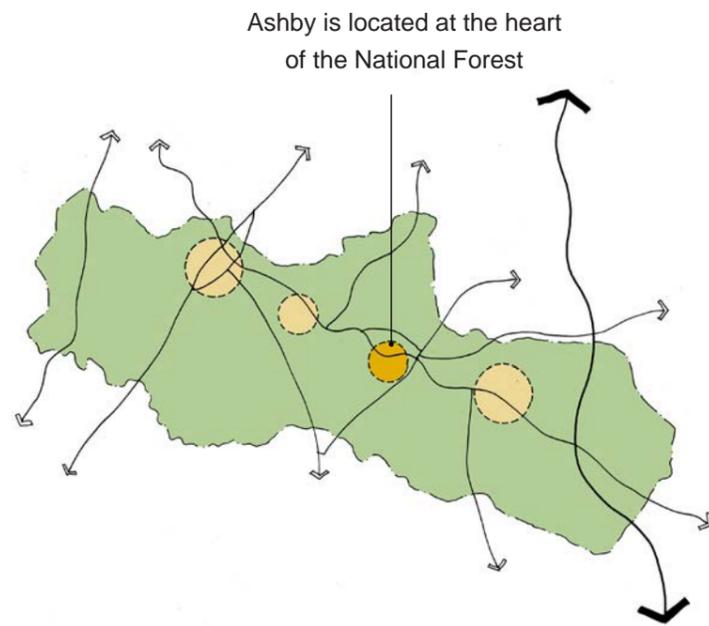
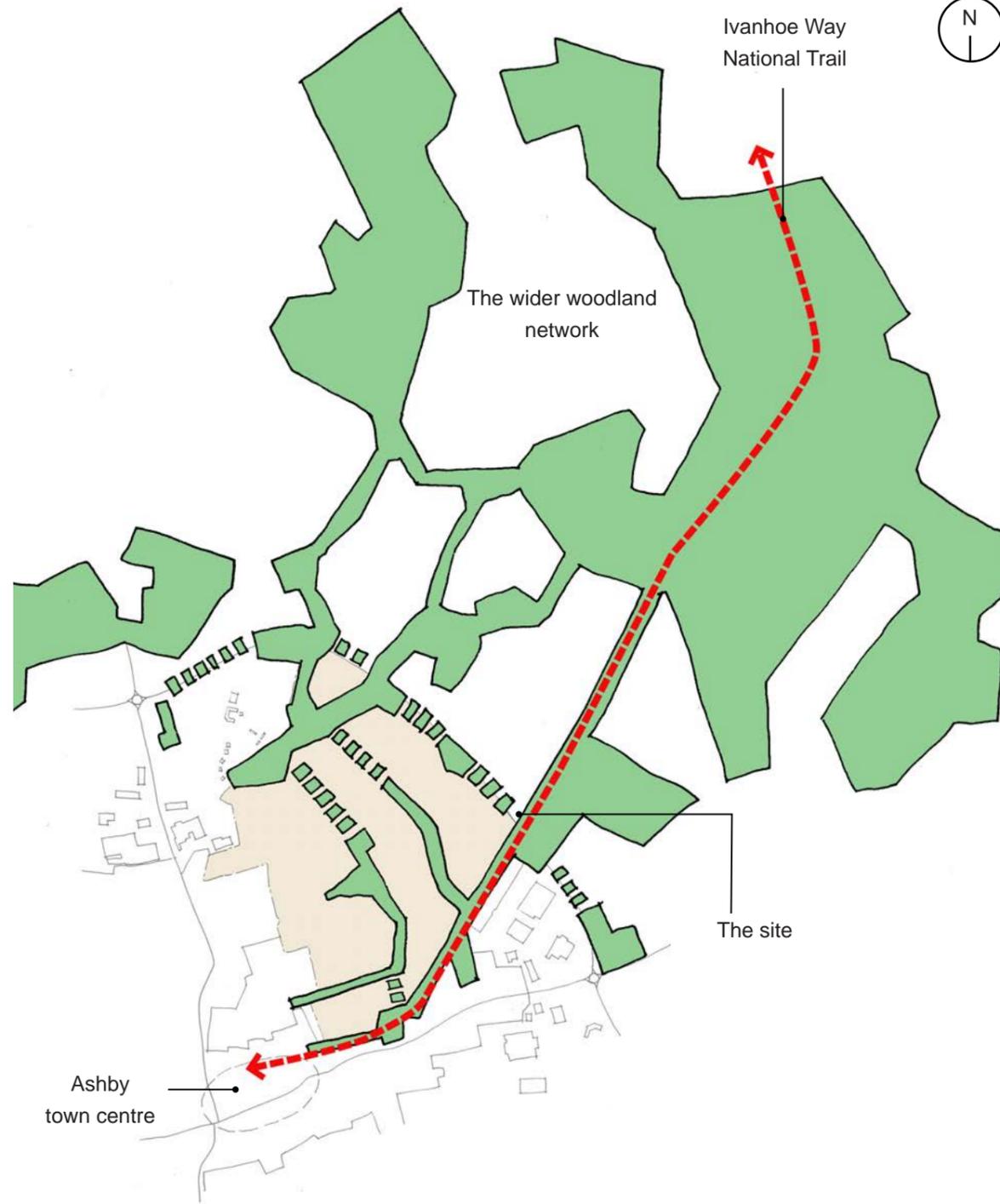
Aerial Photography of the Assessment Boundary

2.3.2 The National Forest

The National Forest Company has identified an area of approximately 200 square miles, the landscape of which it aims to transform by linking the two ancient Forests of Charnwood in the east and Needwood in the west. Ashby is located right at the heart of this designated area, giving the Money Hill site real potential to contribute to this bold and ambitious plan.

By enhancing the existing hedgerows and creating new, connected open spaces, development at Money Hill has the potential to create a series of green corridors linking existing areas of woodland together. Furthermore, there is also the opportunity to create connections beyond the site, linking it into the wider woodland network to the north and east of the A511.

An important green link to be retained and enhanced is along the Ivanhoe Way. This will improve the setting of the National Trail through Ashby.



The Extent of the National Forest

Money Hill Green Infrastructure Concept Plan

The National Forest Company was established in April 1995 and is responsible, through partnership working, for the delivery of the Government-approved National Forest Strategy.

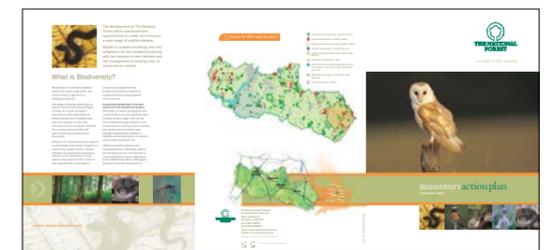
The Company has published a number of guidance documents that set out the National Forest aspiration and requirements for new developments to contribute to the Forest. The following captions show the key documents that have informed the design process of the Money Hill Masterplan.



National Forest Design Charter Guidance Document



National Forest Guide for Developers and Planners

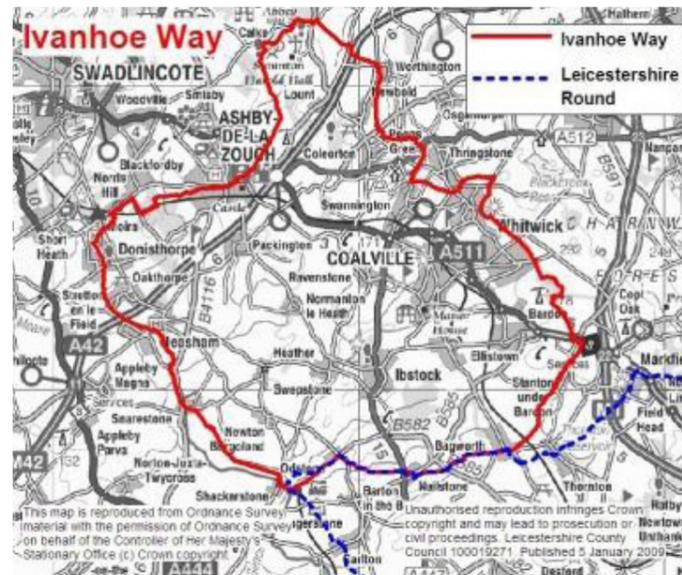


The National Forest Biodiversity Action Plan (2004)

2.3.3 Local Rights of Way and Bus Services

Pedestrian access to the site is good with footways provided on all of the roads that serve it. The site also benefits from existing public rights of way within it, providing connections to the town centre and to destinations further afield to the north and east.

The nearest bus stops to the site are located on Nottingham Road within 150m of the proposed site access locations. The stop is served by routes 3, 9 and 129 providing 2 to 3 buses per hour. In addition to this, routes 2, 7, 8 and 9A are accessible at stops in Market Street some 480m from the site providing 5 to 6 services per hour. The services provide excellent area coverage linking surrounding towns and villages with Ashby town centre, where other services can be accessed connecting with a number of destinations within Leicestershire and beyond.



NWLDG Ivanhoe Way route map

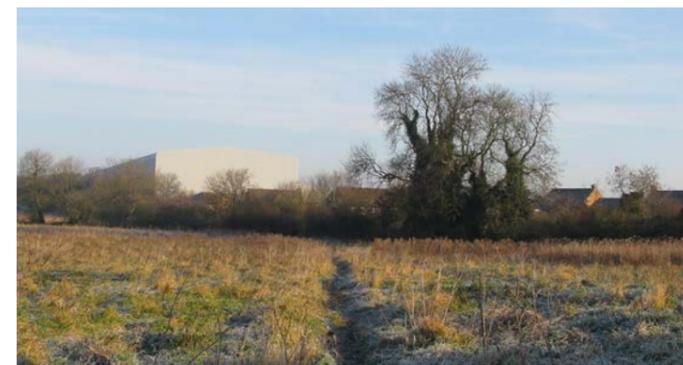
2.3.4 Ivanhoe Way National Trail

The Ivanhoe Way National Trail is a 35-mile circular walk around the north-western area of Leicestershire. The long distance route is divided into 7 sections that can be treated as shorter individual walks. Ashby is suggested as a key start or end point, where walkers can park their cars and use local shops and facilities.

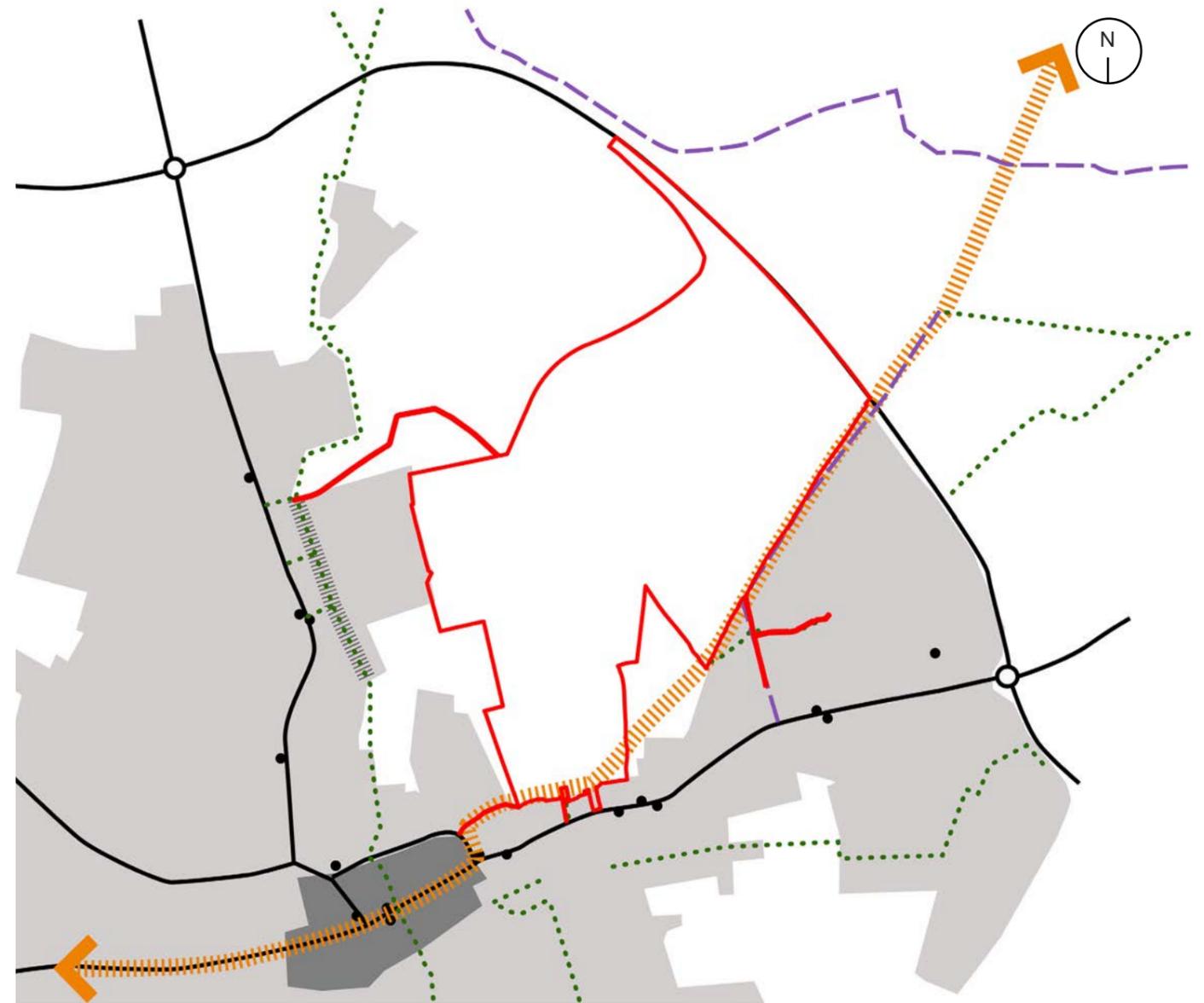
The name 'Ivanhoe' has been linked to north-west Leicestershire since Sir Walter Scott wrote his novel 'Ivanhoe' in 1820. Scott used the castle at Ashby and the surrounding countryside as one of the settings in his book, having become familiar with the area whilst staying at nearby Coleorton Hall.

The Ashby portion of the Ivanhoe Way passes through a mix of urban, rural edge and industrial settings between the town centre and the A511. The trail passes through the urban centre of Ashby and crosses the southern section of the Money Hill site, which currently has a semi rural/urban setting with rear gardens abutting low quality pasture.

Beyond the site heading east, the trail links back into a bridleway that runs parallel to the United Biscuit factory. This industrial interface has low amenity value, and it is only when the trail passes beyond the A511 towards Calke and the Staunton Herold Estate that it regains its rural setting. Through the proposed development, there is the opportunity to improve the setting of the portion of the Ivanhoe Way that runs through the site with a well-designed landscape treatment, emphasising its rural setting.



Poor quality urban and industrial edge of Ivanhoe Way



Existing Rights of Way Plan

- Assessment Boundary
- ↔ Ivanhoe Way National Trail
- Bridleway
- ... Footpath
- Cycleway
- Main Roads
- Bus stop

2.4 Planning Summary

This section provides a summary of the planning and design policy context at the national level, and sets out how the proposed development can be considered to accord with adopted policies and guidance. A more detailed review of planning policy considerations is provided within the separate Planning Statement to accompany this outline planning application.

2.4.1 National Planning Policy Framework

The National Planning Policy Framework (NPPF) was published in March 2012 to provide a framework for plan-making and decision taking.

The primary objective of the NPPF is to increase the delivery of sustainable growth and development. Other key objectives within the NPPF include:

- To significantly boost housing supply, local planning authorities should identify specific deliverable sites (Paragraph 47);
- Local Planning Authorities should approach decision-taking in a positive way to foster the delivery of sustainable development (Paragraph 186);
- Acting as a proactive driver for the delivery of housing needs (Paragraph 17);
- A presumption in favour of sustainable housing development (Paragraph 49);
- The need to deliver a wide choice of high-quality homes (Paragraph 50);
- A plan for housing based on demographic trends (Paragraph 50);
- Identifying a scale and mix of housing that the population will need over the plan period, taking into account migration and demographic change (Paragraph 159); and
- Care and attention to viability and costs in the plan-making process (Paragraph 173).

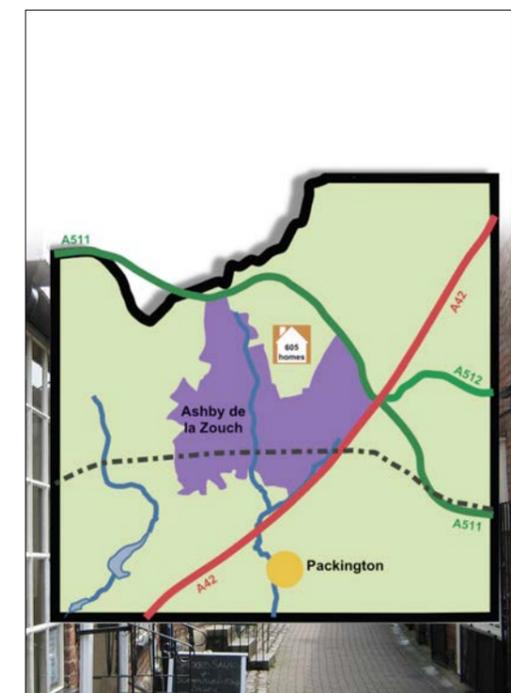
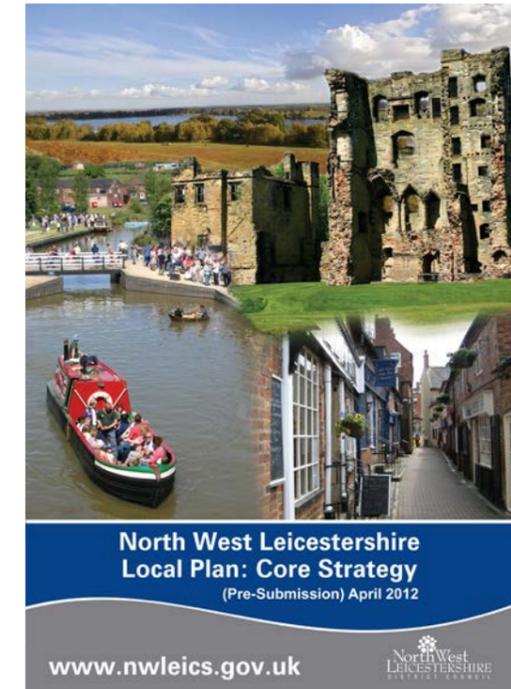
The NPPF is clear that *“housing applications should be considered in the presumption in favour of sustainable development”* (Paragraph 49).

In terms of design, the NPPF states; *“the Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development”. [It] is indivisible from good planning, and should contribute positively to making places better for people”* (Paragraph 56).

The NPPF is also clear, at paragraph 60, that *“planning policies and decisions should not attempt to impose architectural styles or particular tastes, and they should not stifle innovation, originality or initiative through unsubstantiated requirements to conform to certain development forms or styles”*.

The NPPF also states, at paragraph 60, that development proposals should seek to *“promote or reinforce local distinctiveness”*. Despite this, the NPPF recognises, at paragraph 61, that *“the benefits of high quality and inclusive design extend beyond aesthetic considerations”*.

In this context, it is clear that the NPPF is a champion of good design, which is able to make a positive contribution to the lives of local people through the achievement of quality development. The NPPF encourages new development that is informed by a contextual understanding of the site and its surroundings. It is not, therefore, overly prescriptive in its approach to design.



North West Leicestershire Local Plan: Core Strategy

2.4.3 Design Policy

There are a number of policy and guidance documents that have been used to inform the design development of the Money Hill Masterplan. These include;

- The North West Leicestershire Settlement Fringe Assessment (March, 2010);
- Good Design for NWLDC (2013 Draft) SPG (Supplementary Planning Guidance);
- Designing out Crime SPG;
- Development Guidelines SPG.;
- Building for Life 12 and Ourplace Initiative;
- Leicestershire County Council 6 Cs Highway Design Guide;
- Leicester, Leicestershire and Rutland Biodiversity Action Plan (2010-2015);
- National Forest Guide for Developers and Planners;
- National Forest Design Charter; and
- National Forest Biodiversity Action Plan (2004).

The North West Leicestershire Settlement Fringe Assessment (March, 2010) has been a key point of reference to the location of development and open space of the masterplan. The following lists the recommendations if the site was to be developed.

Recommendations or Conditions if site is developed:-

- Retain the wooded character of Gilwiskaw Brook;
- Retain the existing hedgerows and mature trees on site;
- Retain the prominent blocks of woodlands as features within the site;
- Incorporate a network of open spaces through the development integrating existing landscape features such as hedgerows and woodlands;
- Locate open space around existing public rights of way to soften views of development from them;
- Enhance the riparian character of wetlands and watercourses through the site;
- Retain public rights of way within open space with tree planting to help retain some separation from the urban edge;
- Retain and augment the existing woodland planting along the A511, although retaining some gaps to the south to retain views towards the wooded rising edge of Ashby with the Castle and St Helens Church prominent on the highest ground;
- Carefully locate buildings within the site to retain the appearance of a dispersed rising edge with the castle and church prominent on the highest ground;
- Retain the prominence of Cliftonthorpe with a wooded setting with urban development generally screened by landform in views from the north of the site;
- Avoid development on the highest ground which would be prominent and increase the scale and mass of Ashby within views from the wider landscape to the north and south;
- Retain the field pattern within layouts for any development on site;
- Seek to reduce the prominence of the urban fringes particularly around Moneyhill Farm and around the technical college through provision of a landscape structure including planting of small woodland copses;

- Ensure that development appears dispersed within trees and where properties are close to the eastern edge of the site that they appear as smaller clusters of buildings dispersed within woodland rather than a large urban area;
- Seek to reduce the prominence of warehouse development in views from the east through careful siting of woodland;
- Ensure any development within the northern edge of the site, such as along Smisby Road, makes a positive contribution to this entrance into Ashby; and
- Ensure development is not visible beyond the A511 to the north. This is a high point with extensive views across the landscape. If visible it would potentially increase Ashby's visibility within the wider countryside.

2.5.1 Public Consultation

A range of consultations, including discussions with Council Officers, Elected Members, stakeholders and the local community, has influenced the design development of the Money Hill proposals.

A review of the public consultation engaged in by the local community is contained within the Report on Community and Stakeholder Engagement (March 2013) prepared by Green Issues Communiqué. This Design and Access Statement should be read in conjunction with that report.

The stakeholder engagement report considers the pre-application consultation processes undertaken by the applicant, the Money Hill Consortium, and summarises the feedback received during public consultation.

From the outset, the purpose of these consultations has been to engage with local residents and stakeholders in a pro-active, systematic and transparent manner. Local residents and stakeholders were given a variety of ways in which to comment on the proposals, thus enabling a meaningful dialogue to take place. It has also provided the project team with valuable local information and knowledge as well as feedback on the outline Masterplan proposals for both phases of the development project.

The key changes that were made to the Masterplan in direct response to comments received during consultation include:-

- Establishing increased mitigation measures along the boundary between the existing residential properties and the proposed new housing. The measures introduced include increased tree planting and a greater set backs in some locations;
- Proposing construction traffic routes that do not impact on Nottingham Road; and
- Confirming that the phasing of the development will delivers the potential medical centre at an early stage to ensure that this much-needed amenity is secured.



Local residents viewing the exhibition held on 30th November 2012 and 1st December 2012.



1st Consultation Exhibition Boards 30th November - 1st December 2012



2nd Consultation Exhibition Boards 23th January 2013

2.5.2 OPUN Design Review

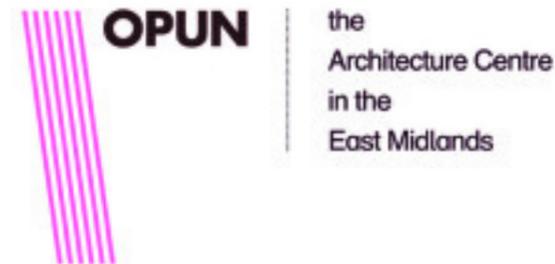
The NPPF encourages new sustainable developments to undergo design review processes to ensure a high standard of design is achieved, stating “Local planning authorities should have local design review arrangements in place to provide assessment and support to ensure high standards of design” (paragraph 62).

For the purpose of the Money Hill Masterplan a design review was arranged with the Architecture Centre in the East Midlands (OPUN). OPUN is a charity that promotes good design practice. It works in partnership with the public, private and community sectors to promote best practice in design and supports the Building for Life 12 assessment.

A design review took place at a stage of design development on 29th January 2013 where the review panel made useful comments across a range of design issues. The main themes where:-

- Character and identity;
- Connections;
- Community facilities;
- The landscape strategy, and
- The relationship with Ashby.

The responses from the OPUN Design Review panel have been addressed in the Building for Life 12 assessment that is included in section 7.2.



OPUN Design Review Exhibition Boards 29th January 2013

2.6 Evaluating Information

Technical baseline studies have been undertaken in support of this planning application and form the Environmental Impact Assessment (EIA), the findings of which are summarised below. Through the consideration of these findings, a number of opportunities and constraints have been identified and have influenced the design of the Masterplan proposals accordingly.

Transport and Access: The Transport Assessment Report shows that the redevelopment site is located in a sustainable location with good footway and cycle links, and a north and south vehicular access point which will serve a new bus route.

Drainage: Surface water run off can be treated on site through a sustainable drainage system. Gravity fall across the site will be used to take foul water and sewage to a pump station close to the watercourse. The pump station will then direct it to a sewer connection at Woodcock Way.

Utilities: All utilities for the site can be serviced from Woodcock Way.

Noise: Noise and air quality surveys reveal that the development of the site will not result in any significant impacts on the local area.

Trees and Ecology: There are a small number of species rich hedgerows and specimen trees on or adjacent to the site. Wherever possible these features will be retained and protected. The nearby River Mease Special Area of Conservation will not be effective by the Money Hill development.

Archaeology: There are a number of archaeologically important hedgerows have been identified within the application site. Development will be set back from these hedgerows, and will be included within the landscape framework.

Effects on Landscape and Visual Character: The overall visibility of the site is relatively well contained due to the existing landform and built up area of Ashby.

Sustainability: The Sustainability Action Plan concludes with a number of sustainability principles that should be through the development, including the promotion of economic activity, the introduction of high quality design, the creation of healthy communities, and adoption of a SuDS landscape strategy.

2.7.1 Design Evolution

There have been a number of Money Hill Masterplan options that have been in the public view. Initially, in 2006 the plan showed a wide ranging approach that included land occupied by Ivanhoe College. At the time it was considered that the college could be integrated into an extensive masterplan which extended up to the A511, and resulted in the provision of a vehicular access to North Street.

1: April 2009:-

- The Masterplan retains the aspiration of vehicular access through the existing site of Ivanhoe College and on to the A511;
- All other connections are considered secondary, either as vehicular connections for small clusters of dwellings, or for cycle and pedestrian use only; and
- Urban structure is determined by access points, water courses and landscape features.

2: September 2010:-

- Masterplan is extended to test the capability of providing 800 dwellings;
- Including consideration of additional circulation loops, a feature park and primary school;
- Estimated vehicle access onto Woodcock Way would be reduced to 130 dwellings;
- Estimated vehicle access onto A511 would be increased to 670 dwellings; and
- Masterplan ensures pedestrian and cycle connectivity between the north and south parts of the plan are maximised.

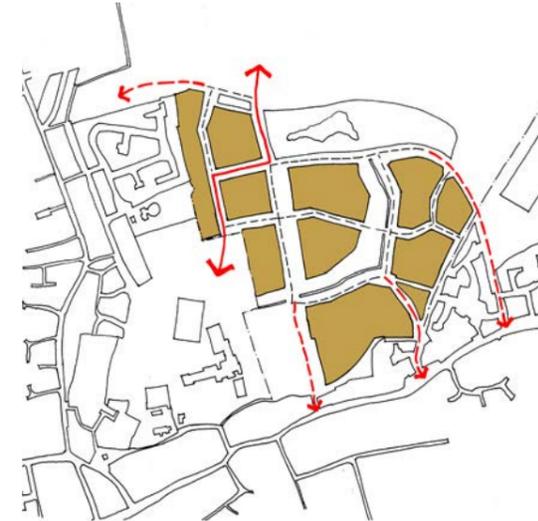
3: March 2013 following consultation:-

- NWLDC Draft Core Strategy state the provision should be made for at least 605 more homes by 2031 to the north of Ashby;
- Inclusion of primary school and areas of open space
- The hedgerows that are considered of archaeological importance are retained;
- Stakeholders require, the inclusion of a medical centre close to the existing town centre; and
- The public preferred, the plan locates new buildings away from existing neighbours dwellings. Revisions considers the amenity of neighbours in general, including distances, heights, land form and buffer planting.

4: April 2015:-

- Due to land becoming unavailable, part of the site at the end of Woodcock Way that would provide access from Nottingham Road was removed;
- A new access point at Nottingham Road is proposed. This will provide vehicular and bus access to the masterplan area. However, private vehicular access will only serve a small proportion of houses and the potential extra care;
- The southern part is revised to integrate a wider range of new access requirements;
- The residential area is amended to accommodate land constraints.

Design Evolution 1: April 2009



Design Evolution 2: September 2010



Design Evolution 3: March 2013



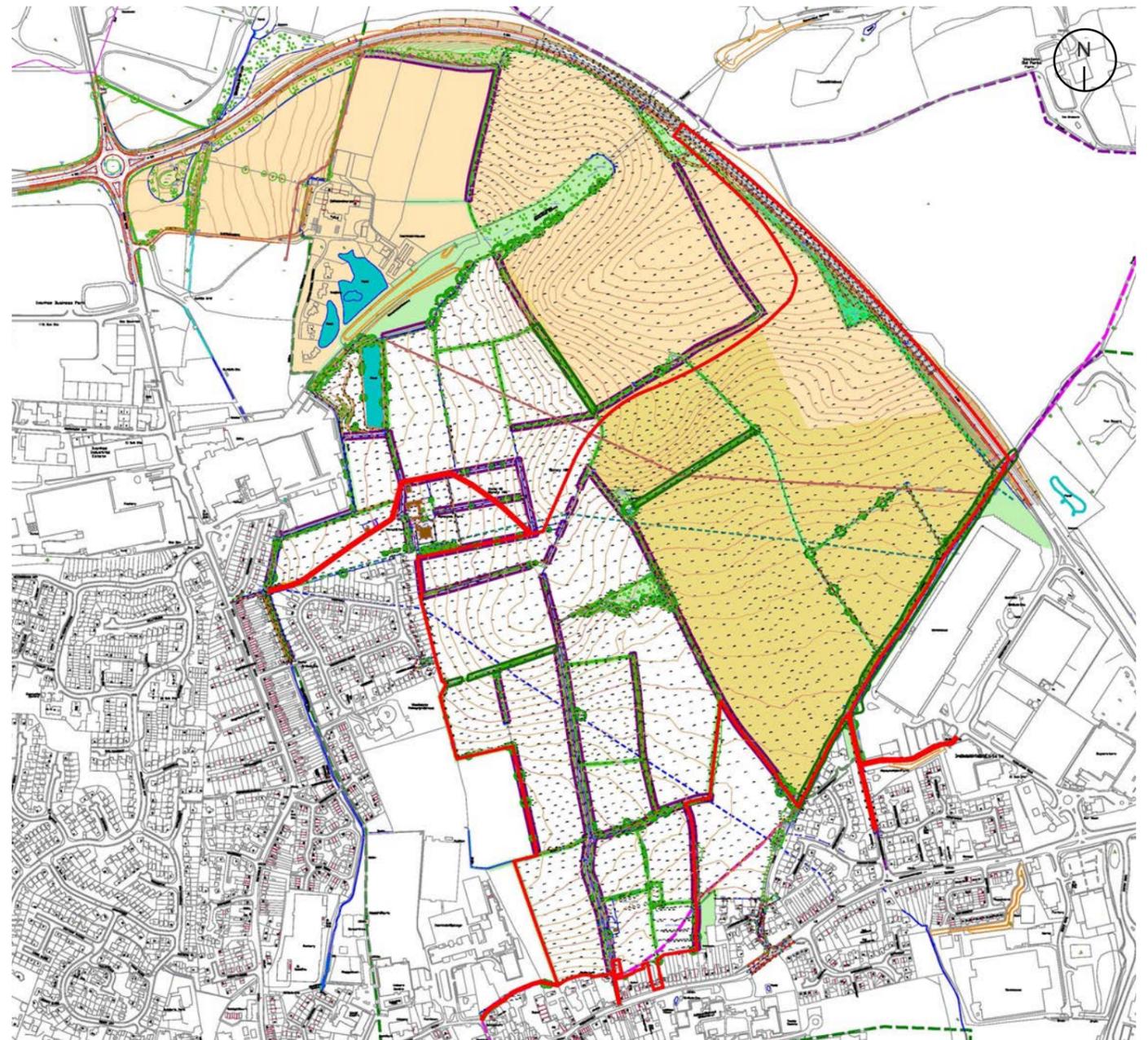
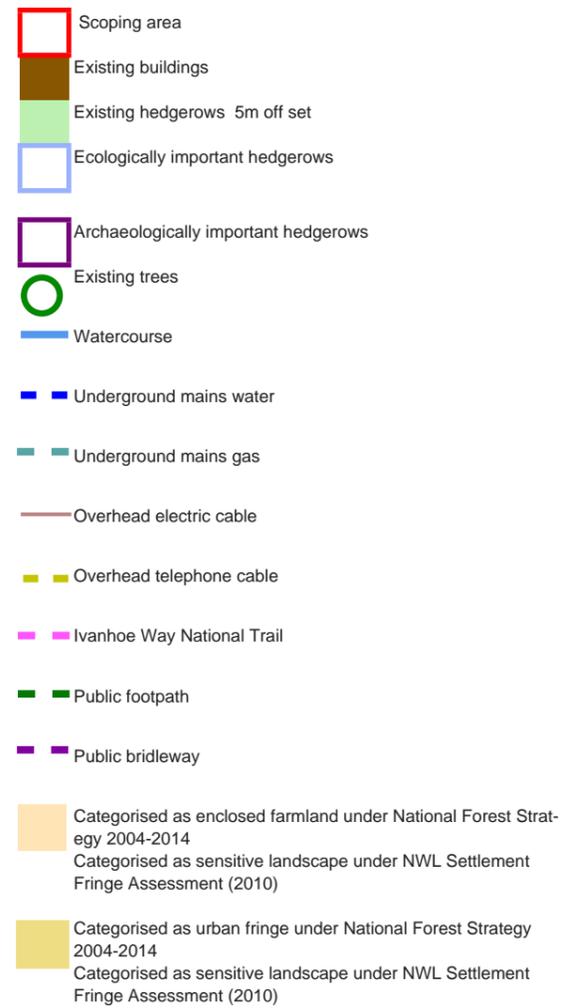
Design Evolution 4: April 2015



2.7 Site Constraints

Based on the technical information contained within these baseline reports and studies the following physical constraints to development have been identified:-

- The historically significant Ivanhoe Way will need to be retained. This will constrain development potential along the southern boundary of the application site;
- Development will need to be set back from the archaeologically and ecologically important hedgerows that run north to south through the site and along part of the watercourse corridor;
- Development will also need to be set back from the watercourse and from low-lying land to allow for the successful implementation of a SuDS network;
- Specific key mature trees will need to be retained and integrated into the landscape proposals. Each of these will require a root protection zone to ensure that neither the development itself nor the construction work required to create it will cause them any damage;
- The new streets and buildings will need to be designed to accommodate the existing contours of the site;
- The minor agricultural buildings that currently exist on the site will need to be removed before construction begins;
- The existing mains water and gas routes will need to be redirected through the development; and,
- Development will need to avoid the sensitive areas of landscape to the north-east, adjacent to the A511.



Constraints Plan

3. DESIGN PRINCIPLES

3.1 Rationale

The rationale behind the design proposals for the Money Hill site can be structured into the following six themes:-

1. Green Infrastructure;
2. Landform and Drainage;
3. Proximity to the Town Centre;
4. Managing the Car and Allowing for Buses;
5. Pedestrian Access and Connectivity; and
6. Development Interfaces.

The following section of this Design and Access Statement discusses each of these themes in turn.

3.1.1 Green Infrastructure

In terms of Green Infrastructure, the overarching aspiration for the Money Hill development is to link it into the significant areas of woodland that lie beyond the site, thus contributing to the vision of the National Forest Company (please refer to 2.4.2).

One significant contribution that the Money Hill development will make is to ensure that the existing areas of copse and woodland on the site are retained, enhanced and linked by connected hedgerows. This will create green corridors that have the potential to improve site biodiversity by providing multifunctional routes for migrating wildlife as well as semi-natural walking and cycling routes.

There are also a number of key mature trees on the site that will be retained and complemented by new planting. They will create and inform the character of the new neighbourhoods that develop around them.

Within the Masterplan proposals, the layout of streets and houses follows the alignment of the proposed green corridors and, where possible, the green corridors including Ivanhoe Way are integrated into the design of the public realm in order to make them easier to manage in the long term.



3.1.2 Landform and Drainage

The existing landform of the site is characterised by a series of gentle slopes, with the lowest point found to the western end of the watercourse and the highest near to the A511.

Rainwater will be managed where it falls on site in order to minimise the impact of the development on the existing flood burden within Ashby. This will be achieved through the use of swales and attenuation areas in the lowest lying areas, higher density development in discrete locations and reduced amounts of development on the high level ground. As part of this approach, development alongside the watercourses will provide an informal network of linked open spaces.

The biodiversity of the watercourses will be enhanced through the addition of swales and basins, which will in turn increase ecological opportunities within the site and create a strong framework for the landscape design.

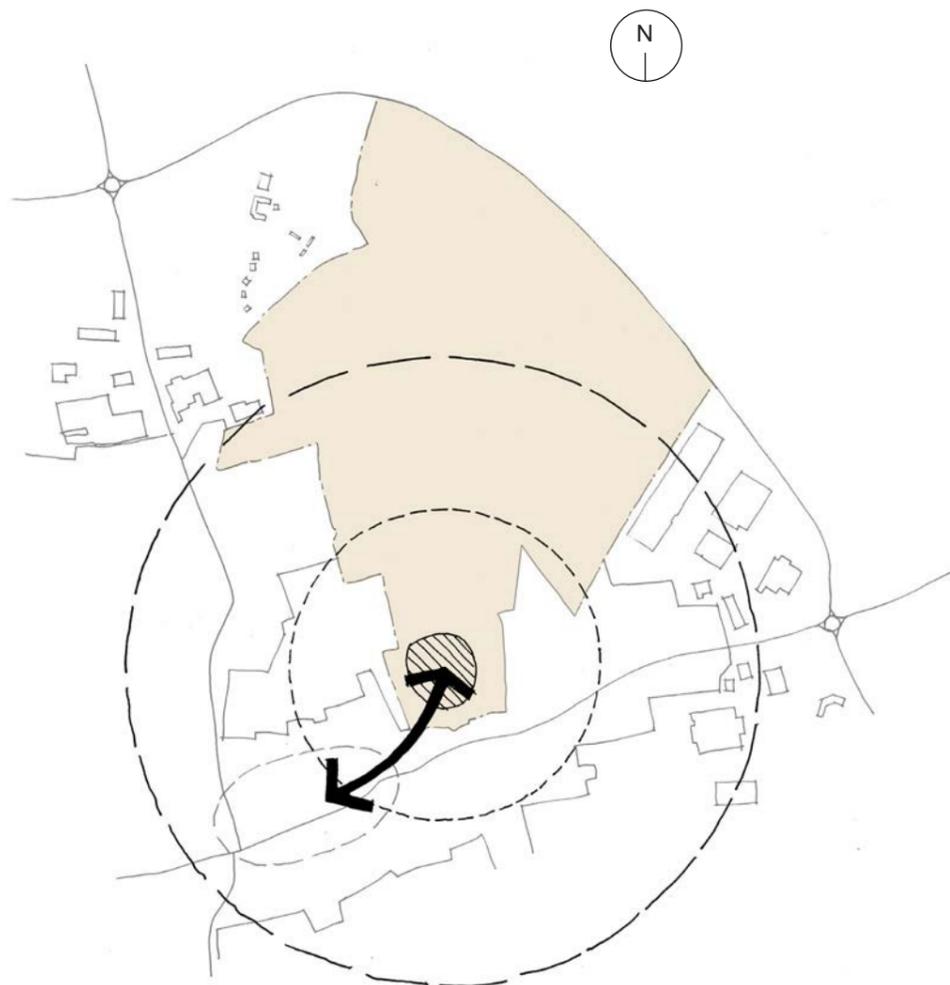
The contours of the wider site will allow the streets north of the watercourse to be orientated well in relation to the movement of the sun, thus creating the potential to capitalise on passive solar gain and natural sunlight.



3.1.3 Proximity to the Town Centre

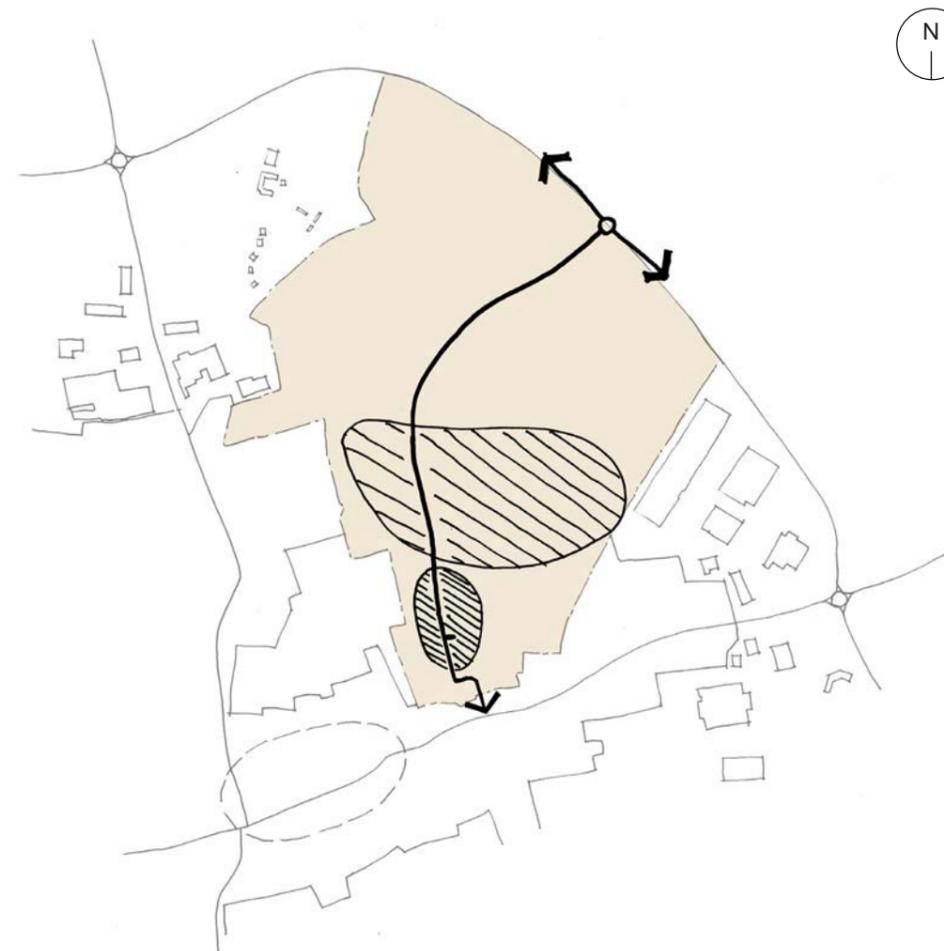
As part of the Money Hill development, a new local centre area will be created to support Ashby's town centre vitality objectives and become a new asset for the town. The new local centre area will be located in close proximity to the existing town centre and will complement the services that are already provided. For example, two proposed uses that are sympathetically associated with the town centre are the potential extra care facility and potential community hall.

As the Money Hill site falls within a 5 minute walking distance from the town centre, new residents could easily access the facilities and services on offer by walking, cycling or taking a bus. A bus gate will control the bus route proposed through the Money Hill development in order to ensure that it does not encourage the use of private motorcars.



3.1.4 Managing the Car and Allowing for Buses

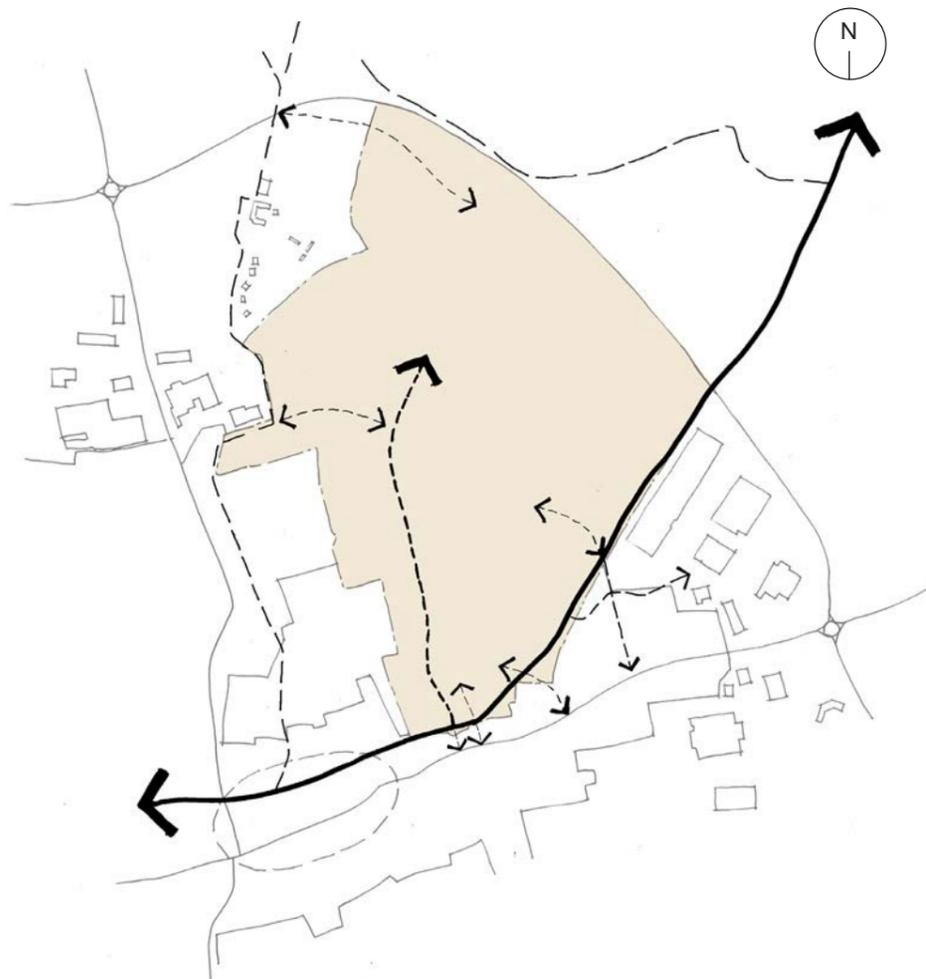
Car access is from a new access point in Nottingham Road and the A511. Bus access will also be from Nottingham Road and the A511. However, the new access point will only serve vehicular access to a small number of houses and the potential extra care. A bus gate would be incorporated to the north-west of the new access point to restrict the flow of cars.



3.1.5. Pedestrian Access and Connectivity

The Money Hill development is envisioned as a walkable community, the design of which supports the use of local amenities and public transport and aims to minimise car dependency. In order to achieve this, the Masterplan proposes the creation of welcoming, convenient, well over-looked and safe pedestrian and cyclist routes, with the intention that walking will become the first choice mode of transport for accessing the surrounding neighbourhoods and parkland.

Existing public rights of way within the site will be retained and integrated into the design solution. They will be enhanced and complemented by new pedestrian routes that will connect the new and existing neighbourhoods.

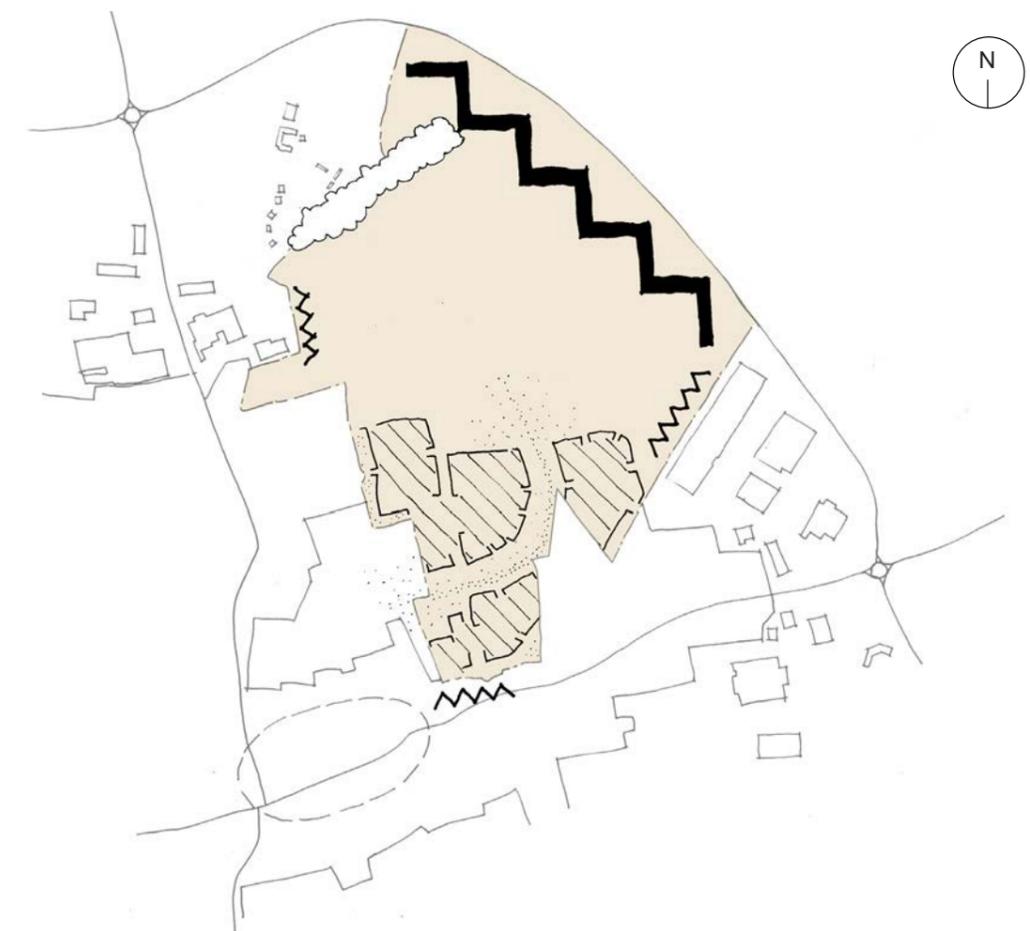


3.1.6. Development Interfaces

The Masterplan proposals respond sensitively to the different edge and boundary conditions that currently exist around the site. The edge conditions that require special consideration are:-

- The A511, which is on raised ground and therefore has the potential to be a source of nuisance noise and visually intrusion;
- The industrial edges of the site, which are unsightly and could cause a nuisance if residential development were to be located next to them;
- The woodland edges associated with the disused railway line;
- The edges that abut existing residential properties; and
- The public rights of way which will be left in situ.

The proposed Masterplan adopts a specifically tailored approach to each of these edge conditions in order to ensure that the development fits well within the town as a whole.



3.2 Design Principles

The following section outlines the design principles that have informed the generation of the Masterplan. These principles are based on the key issues to consider identified within Guidance on information requirements and validation (CLG, March 2010) and include:-

- Use;
- Amount and Scale;
- Block Structure;
- Landscape; and
- Character.

Diagrams demonstrating how the Masterplan proposals respond to the local area and existing physical constraints accompany each of the following descriptions of the design principles.

Two other key principles behind the design of the Masterplan are site drainage and access and movement. However, as these themes are considered particularly important to the success of the development, they have been given dedicated chapters within this Design and Access Statement. Please see Sections 4 and 5 for further detail.

3.2.1 Uses within the context of Ashby

The Masterplan proposes to deliver a vibrant mix of uses including residential development, a primary school, a potential medical centre, local shops, a potential nursery school, a park, areas of natural landscape including balancing ponds, agricultural land and woodland buffers.

The siting of each of these different uses has been carefully considered in order to ensure that the new development integrates well with the existing uses near to the site. For example, where important town centre uses are proposed within the site, they are located close to the existing town centre in order to ease accessibility.

The majority of the proposed residential development is arranged in the central area of the site, with more clustered near to the existing properties along the southern and western boundaries. Landscaped areas are proposed at strategic locations across the site in order to create landscape buffers, enhance the settings of existing trees and create green corridors around the existing hedgerows.

In order to successfully absorb surface water run-off, drainage swales and attenuation areas will be located on the low lying land close to the watercourse, near the drainage ditch on the south-eastern boundary and within the north-western residential area.

- Assessment boundary
- Residential development
- Extra care/ residential/ community hall
- Local centre areas
- Drainage areas
- Open space / tree planting areas
- Car parking
- Primary School
- Agricultural land



Land Use Principle Plan

3.2.2 Uses responding to Ashby's needs

Parking

The mixed use character of Ashby town centre generates vitality centred on Market Street. There are a number of bus services and small scatters car parks that serve the town centre. However, public consultation has identified that there is a potential under provision of car parking leading to a subsequent loss of some vitality to food stores on the edge of town.

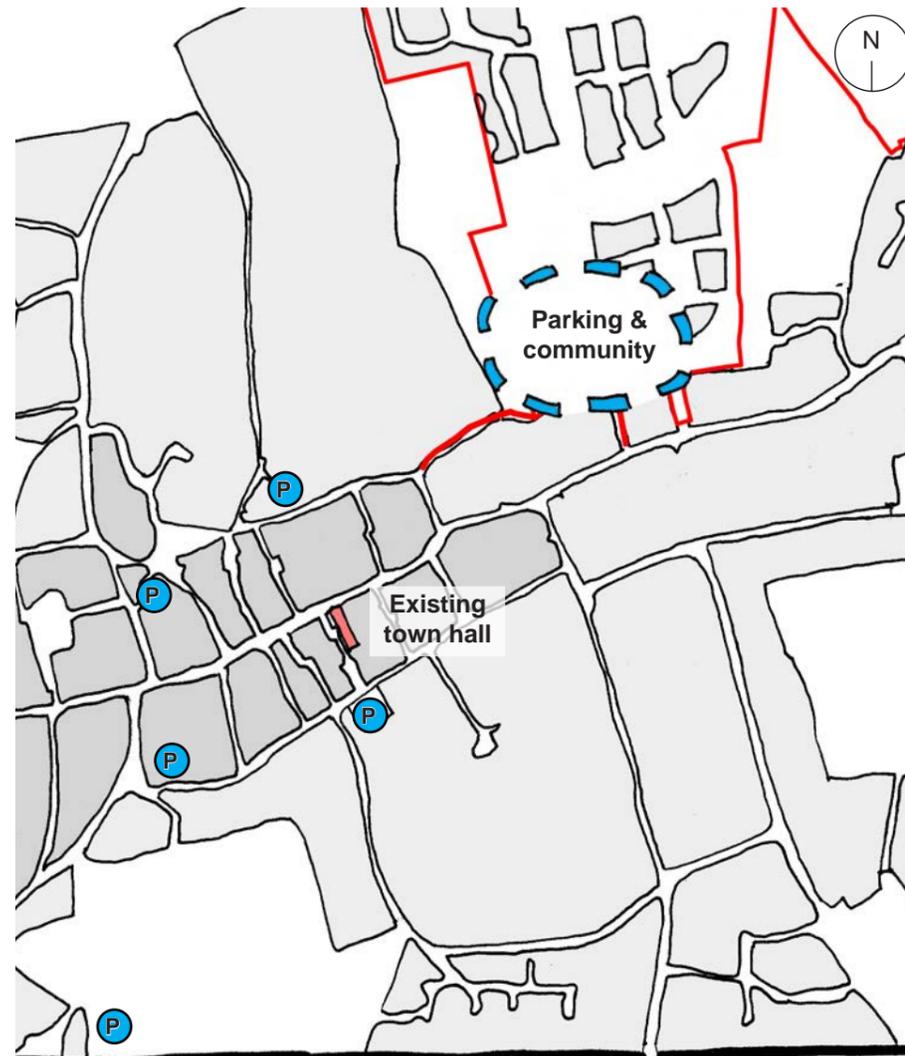
There is therefore an opportunity for the Money Hill masterplan to contribute additional car parking towards the network of existing parking provision.

Community facilities

The accommodation of community events and facilities is also considered to be an important town centre use. Until it underwent a recent change of use, Ashby's original Town Hall was the place where community functions would take place. However, following this change of use, there is currently no space for the community to hold events. As a result, a potential community hall is proposed within the Money Hill Masterplan. Whether this forms part of the southern neighbourhood, or the local centre area of the school, will be subject to further analysis.

Ivanhoe Way

Ivanhoe Way will connect the town centre with the parking and community facilities. There is an opportunity to improve on this pedestrian and cycle link.



- Assessment boundary
- P Existing car parks within the town centre



Existing view looking west along Ivanhoe Way towards proposed community & parking



Sketch illustration of potential improvements to Ivanhoe Way pedestrian and cycle link to the proposed community and parking area

3.2.2 Amount and Scale

For a detailed breakdown of the quantum of development, please refer to the 'Amount' and 'Scale' Parameter Plans found in sections 6.7 and 6.8 of this document.

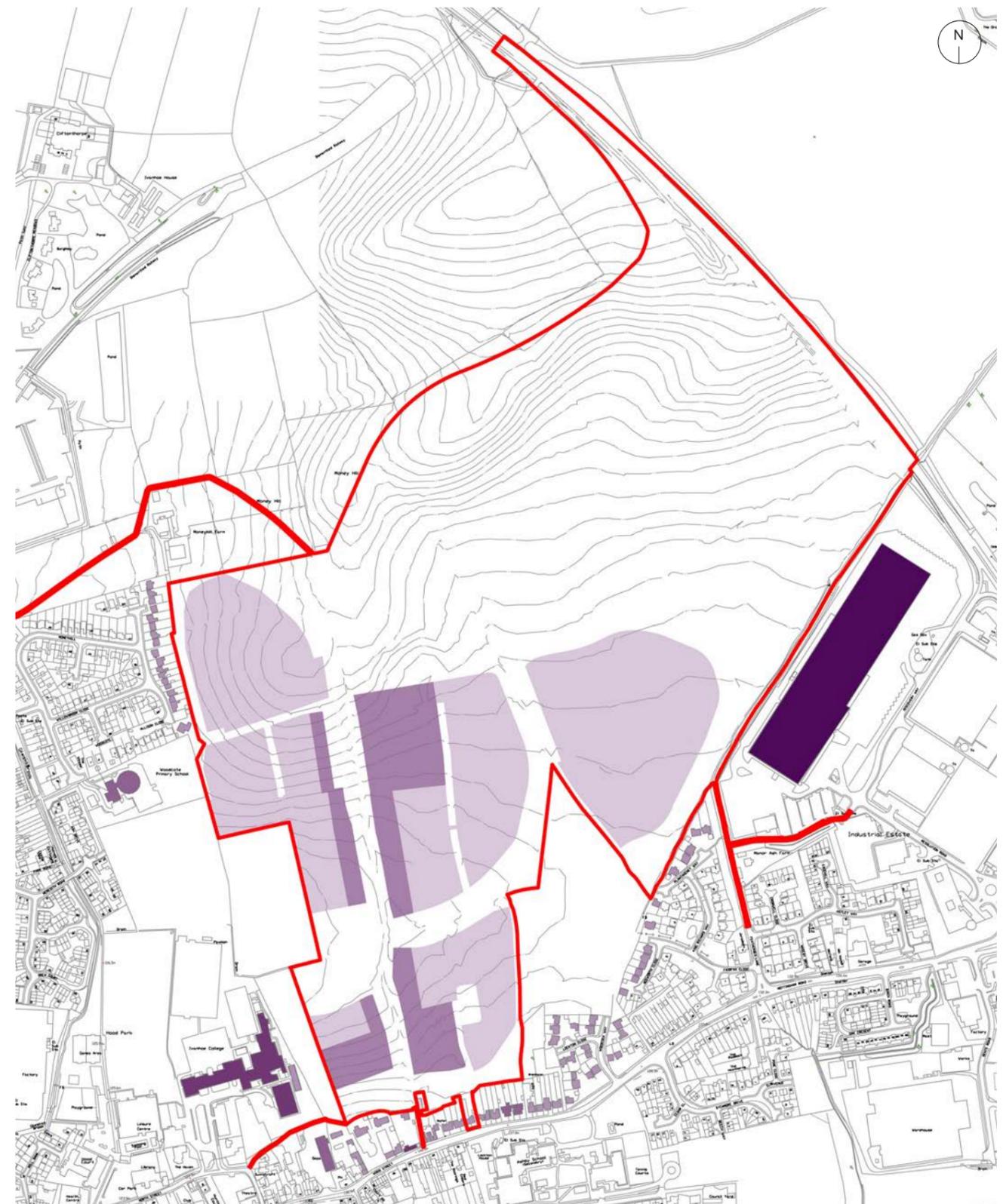
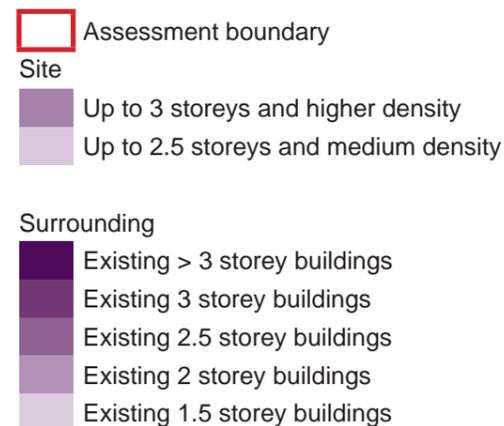
The appearance and impact of proposed building heights will be significantly affected by the ways in which they are distributed across the landscape and topography of the site. Therefore residential streets will generally follow the contours of land and the drainage strategy.

The proposed building heights also respond to the location of the existing neighbouring properties. In order to sensitively respond to these existing buildings, the Masterplan ensures that none of the new houses close to the site boundaries will be any taller than two storeys. In some particularly special locations, the ground levels will be reduced to create lower building platforms close to existing houses.

Civic buildings such as the potential medical centre and school will require greater ceiling heights to accommodate their functions successfully. These buildings will therefore be limited to two storeys in height.

The key scale and density principles behind the Money Hill proposals are to:-

- Deliver lower density development close to the open spaces;
- Deliver higher density development close to local centre area;
- Introduce buildings of increased scale along the main north-south corridor;
- Use key buildings with greater prominence to signify the entrances to the neighbourhood;
- Ensure that the density characteristics of the new development are similar to those of the surrounding residential neighbourhoods; and
- Use a mix of dwelling types with different building widths, depths and height to vary street character and respond to land uses and open spaces including detached, semi detached, terrace and courtyard house types.



Amount and Scale Principle Plan

3.2.3 Layout

The Masterplan sets out a clear approach to the hierarchy of streets and spaces based on a robust block structure, a clear definition between public and private spaces and the flexibility to accommodate a mix of different uses. The key components behind the layout of the Money Hill proposals include:-

- Block Structure;
- Perimeter Blocks;
- Active Frontages; and
- Sustainable Building Design

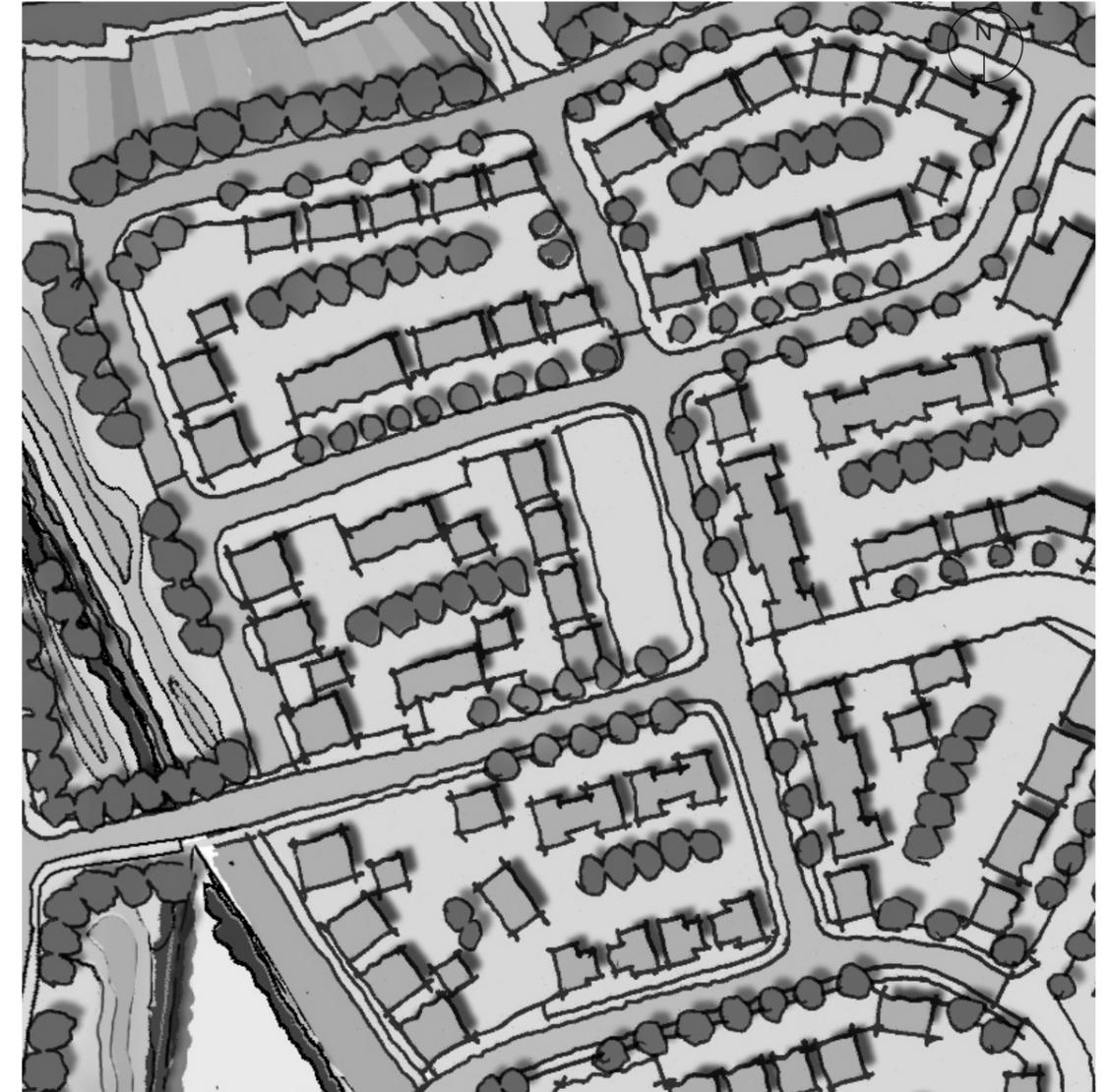
Block Structure

The block structure of the Masterplan is comprised of perimeter blocks created by buildings that front onto the public realm and benefit from secure private gardens to their rears. This block structure supports a positive street layout with front door access onto the public realm encouraging activity and providing natural surveillance. Having private garden space within the perimeter blocks increases the security of the properties and creates clear definition between public and private space.

The sizes of the blocks varies depending on a number of factors including the preservation of hedgerows, the location of pedestrian routes to and from the town centre and the distribution of other uses. Uses such as the school require increased block sizes.

The movement hierarchy has also influenced the block layout, with more continuous building frontages used to define primary routes and semi continuous frontages proposed for quieter residential streets.

The block structure has also been orientated so that, wherever possible, buildings have good solar orientation. For further explanation please see page 30.

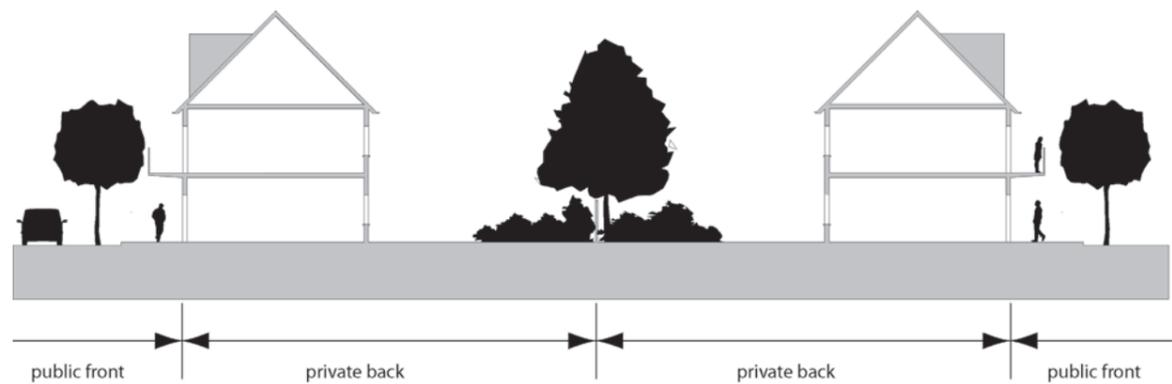


Typical layout illustrating a perimeter block with active frontages, a range of street types, and pedestrian routes.

Perimeter Blocks

The perimeter block arrangement proposed for Money Hill allows sufficient flexibility to accommodate a range of different uses and creates clear definition between public and private spaces. The perimeter blocks are arranged so that the buildings have public outward looking frontages and secure private rear gardens, or in some locations, rear parking areas. This arrangement should ensure that the streets are treated as shared public spaces.

To ensure adequate privacy distances there should be no less than 20m between habitable rooms across rear gardens. Where proposed development is intended to back onto existing residential development, the distance between properties must be no less than 22m.



Perimeter block cross section showing back to back private rear gardens and public frontages

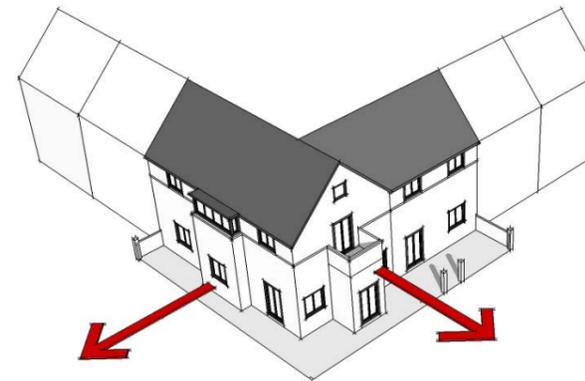
Active Frontages

It is important that all buildings front onto streets and open spaces in order to create a sense of public ownership of them. At the Money Hill development, the detailed design of ways in which the proposed buildings address the streets and open spaces should also be used to create a rich sense of place and character. For example, front doors should face onto all of the streets and open spaces and the most social rooms within the houses should generally be located towards the front of the buildings. This will encourage natural surveillance of the public realm, reduce the fear of crime and generate a sense of community. Furthermore, where it is appropriate, corner buildings with windows on both sides should be used to address two routes simultaneously. This will ensure continuous and semi continuous frontages.

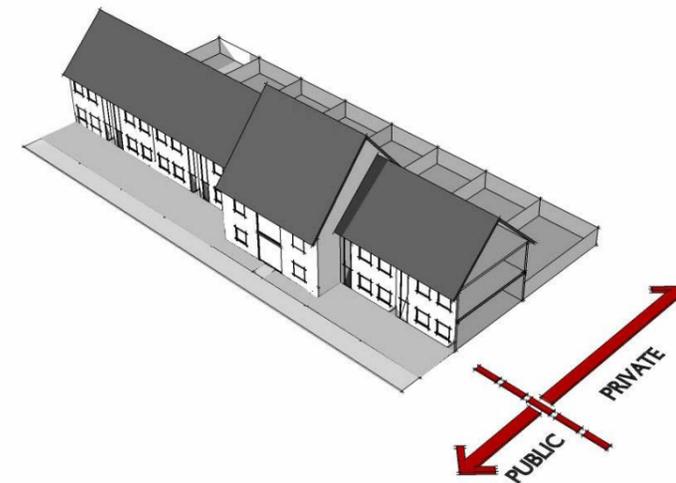
All of the community and commercial buildings proposed for Money Hill will also be required to provide active frontages on to the public realm. This is particularly important for the local area locations of the potential medical centre and the potential community hall. With regard to the mixed use buildings and apartment blocks, both the ground floor and upper floor uses must all be accessed from the front of the building.

The key principles behind achieving a successful block structure at Money Hill are:-

- Creating buildings that define clear and legible routes;
- Using active frontages to clearly define public and private spaces;
- Enabling good natural surveillance of the public realm; and
- Designing building arrangements that provide character and interest to the street.



Corner buildings to have dual aspect



Clear definition of public and private space

Designing out Crime

The Money Hill Masterplan promotes social sustainability to support of the delivery of a wide choice of high quality homes, a requirement for good design and the promotion of healthy communities. This includes a preference for a reduction of crime and disorder and an increase in health and wellbeing.

For designing out crime principles refer to “Safer Places: The Planning System and Crime Prevention” (ODPM 2004) so that Secured by Design standards can be achieved. Safer Places identifies seven attributes of safe and sustainable places, including:-

- Places with well-defined routes, spaces and entrances that provide for convenient movement without compromising security;
- Places that are structured so that different uses do not cause conflict;
- Places where all publicly accessible spaces are overlooked;
- Places that promote a sense of ownership, respect, territorial responsibility, and community;
- Places that include necessary, well-designed security features;
- Places where the level of human activity is appropriate to the location and creates a reduced risk of crime and a sense of safety at all times; and
- Places that are designed with management and maintenance in mind, to discourage crime in the present and the future.

Sustainable Building Design

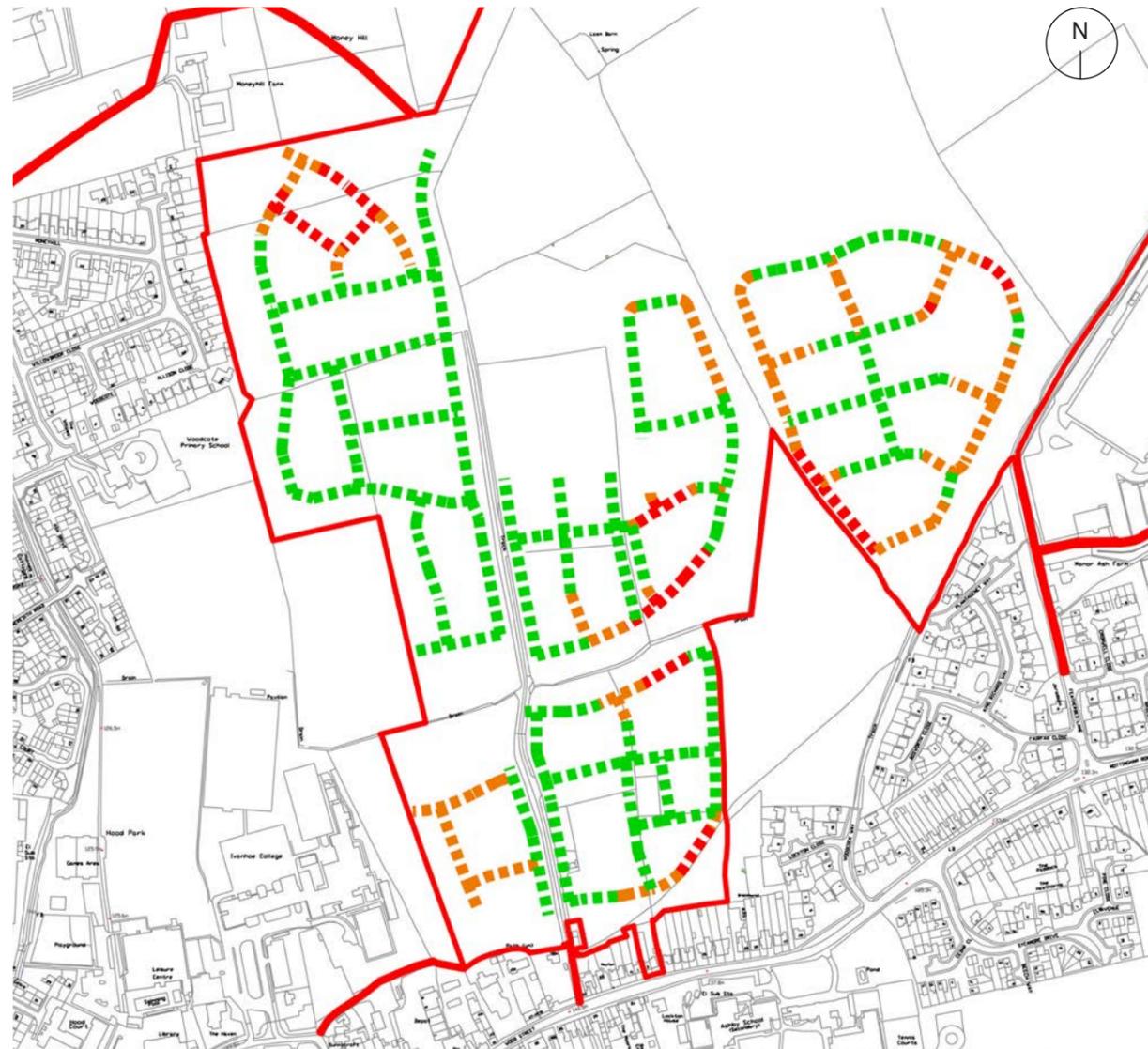
As the Money Hill Masterplan is delivered, all of the buildings will be designed to comply with the building regulations at the time of submission. Over and above the base line of the building regulations, additional measures such as careful orientation and roof design will enhance the sustainability of the development as a whole.

Orientation

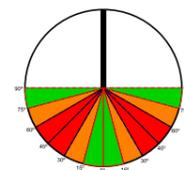
To balance best practice urban design and sustainability principles is complex, particularly when reconciling passive solar orientation with other objectives such as the retention of hedgerows, enhanced public rights of way and a sound block structure. However, the proposed layout of the streets in the Money Hill Masterplan provides the opportunity to allow for good passive solar gain and natural ventilation, particularly when dwellings, habitable rooms, roofs and windows are oriented to create an optimum balance.

As indicated in Section 3.2.3 of this Design and Access Statement, the proposed block structure for Money Hill provides good solar orientation, with the majority of the residential streets on an east-west axis, thus maximising the number of south facing roofs where thermal panels or photo-voltaic tiles could be located. Buildings on streets that follow a north-south axis can also benefit from such measures if additional care is taken with roof form and the density of development.

To support this approach, a solar orientation exercise has been undertaken to analyse the potential for dwellings to accommodate solar panels. The resulting plan and table show that approximately 70% of all of the streets in the proposed Money Hill Masterplan have good to excellent solar orientation.



Excellent solar orientation	Streets where dwellings will have excellent solar orientation through either: Terraces with pitched roofs facing south; or Detached dwellings where gables face the street have pitched roofs facing south.
Good solar orientation	Streets where dwellings will have good solar orientation through either: Terraces with pitched roofs facing south; or Detached dwellings where gables face the street have pitched roofs facing south.
Poor solar orientation	Streets where other design requirements are needed but this produces a poor solar aspect: Joined dwellings aligned on a north - south axis; or Special urban design settings, or Streets connection north to south.

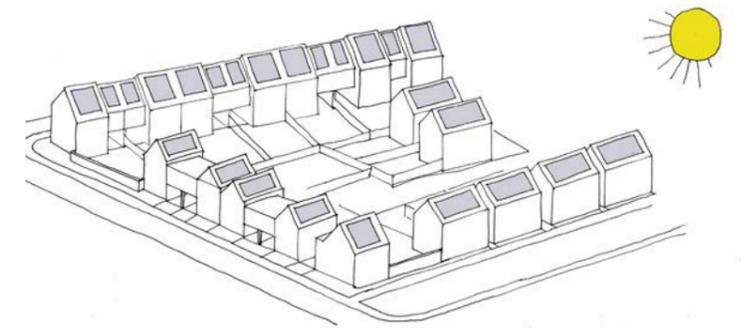


- Assessment boundary
- Excellent solar orientation
- Good solar orientation
- Poor solar orientation

Solar Analysis Plan and Table

Large roof spaces

Public buildings such as the community hall, the school and the potential medical centre present the opportunity to include green or 'living' roofs. These consist of a growing medium with a special mix of grasses and wild flowers over a waterproof layer. Such roofs will not only contribute towards the SuDS strategy by absorbing rainwater and slowing run off, they will also support wildlife conservation by increasing the biodiversity habitats found at Money Hill.



Solar Analysis Diagram

3.2.4 Landscape

As stated in the vision statement (see 1.2), Money Hill will have woodland identity reflecting its central location within the National Forest. The woodland identity will be reinforced by retaining and enhancing existing landscape features such as the watercourse corridor, Ivanhoe Way, important hedgerows and copse areas.

There are a number of landscape components that have informed the principles behind the landscape framework including:-

- Pedestrian routes;
- The hedgerow network; and
- The watercourse.

In relation to pedestrian routes, the landscape setting of Ivanhoe Way will be enhanced and improved through the creation of a new linear open space with a meadow character between Ashby town centre and the A511. The hedgerows retained across the site will provide attractive green routes through the development connecting smaller areas of green space within the proposed residential areas.

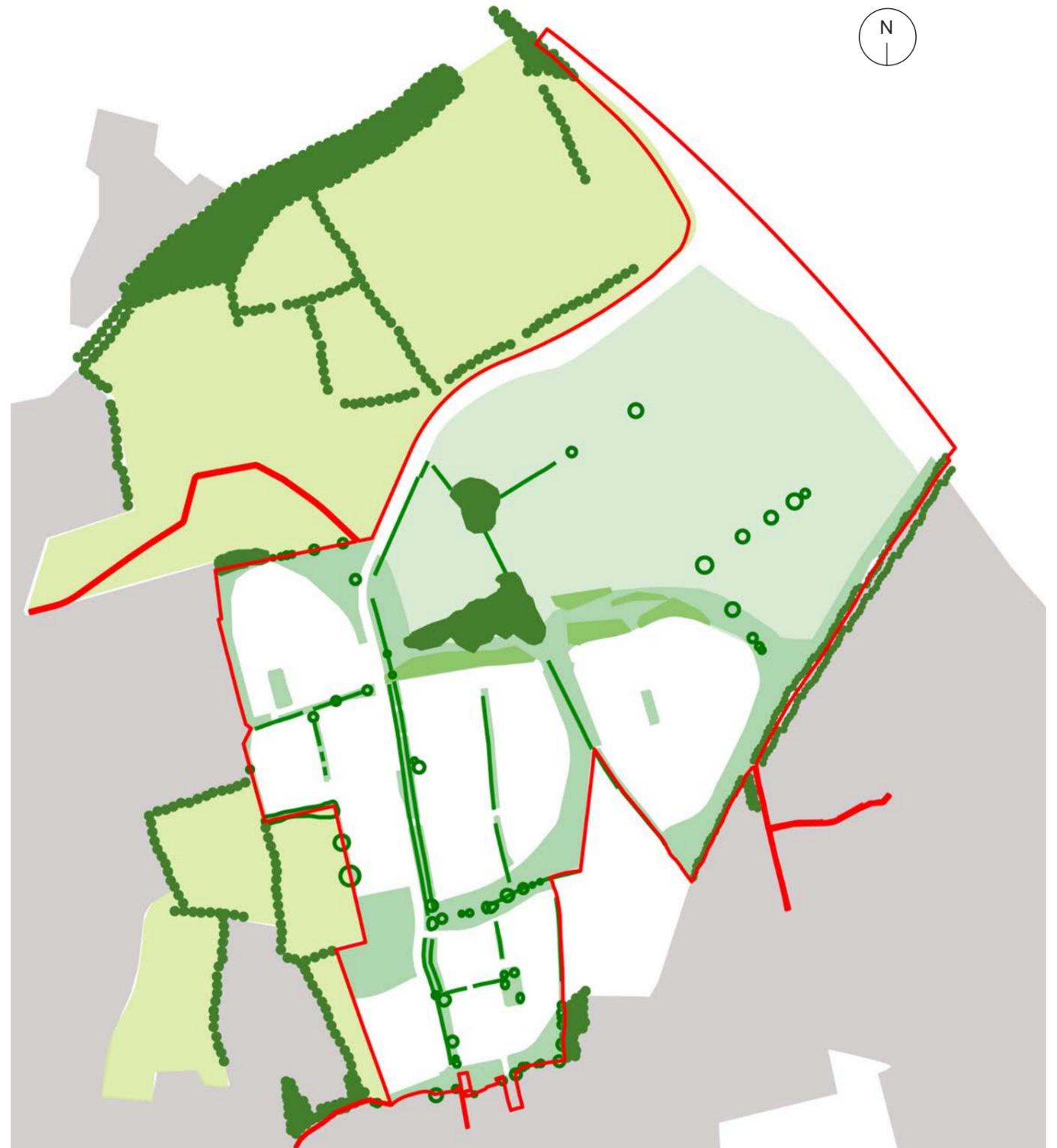
The existing valley character of the southern section of the watercourse corridor will provide a key landscape feature for the Money Hill Masterplan. In this location, a series of multifunctional spaces will be created, incorporating sustainable drainage features and new amenity spaces for all to enjoy. The watercourse itself will also provide an important ecological corridor for local wildlife, thus improving the site's biodiversity (see Leicester, Leicestershire and Rutland Biodiversity Action Plan 2010-15 for guidance).

In accordance with the National Forest Company's Guide for Developers and Planners' the proposed Masterplan delivers more than the recommended 30% of development area as new open space and woodland planting. New woodland planting along the north-eastern boundary will also help mitigate the negative visual prominence of the United Biscuit warehouse.

The key principles necessary to achieve a high quality landscape framework at Money Hill are;

- To deliver at least 30% of the development area as open space and woodland planting;
- To enhance the watercourse environment with swales and reed bed areas;
- To create a strong woodland character in keeping with the National Forest;
- To retain and enhance all of the important trees and hedgerows on the site and to allow these to influence the urban grid of street and houses;
- To provide additional squares and greens that continue the sequence of urban landscape spaces;
- To enhance Ivanhoe Way as an important National Trail route; and
- To provide woodland belt planting to mitigate the impact of the development on long views.

-  Assessment boundary
-  Open space areas
-  New tree planting
-  Agricultural land within assessment boundary
-  Retained hedgerows
-  Retained trees
-  Retained and existing tree groups
-  Existing hedgerows beyond the application boundary
-  Open space beyond the assessment boundary
-  Built form beyond the assessment boundary



Landscape Principle Plan

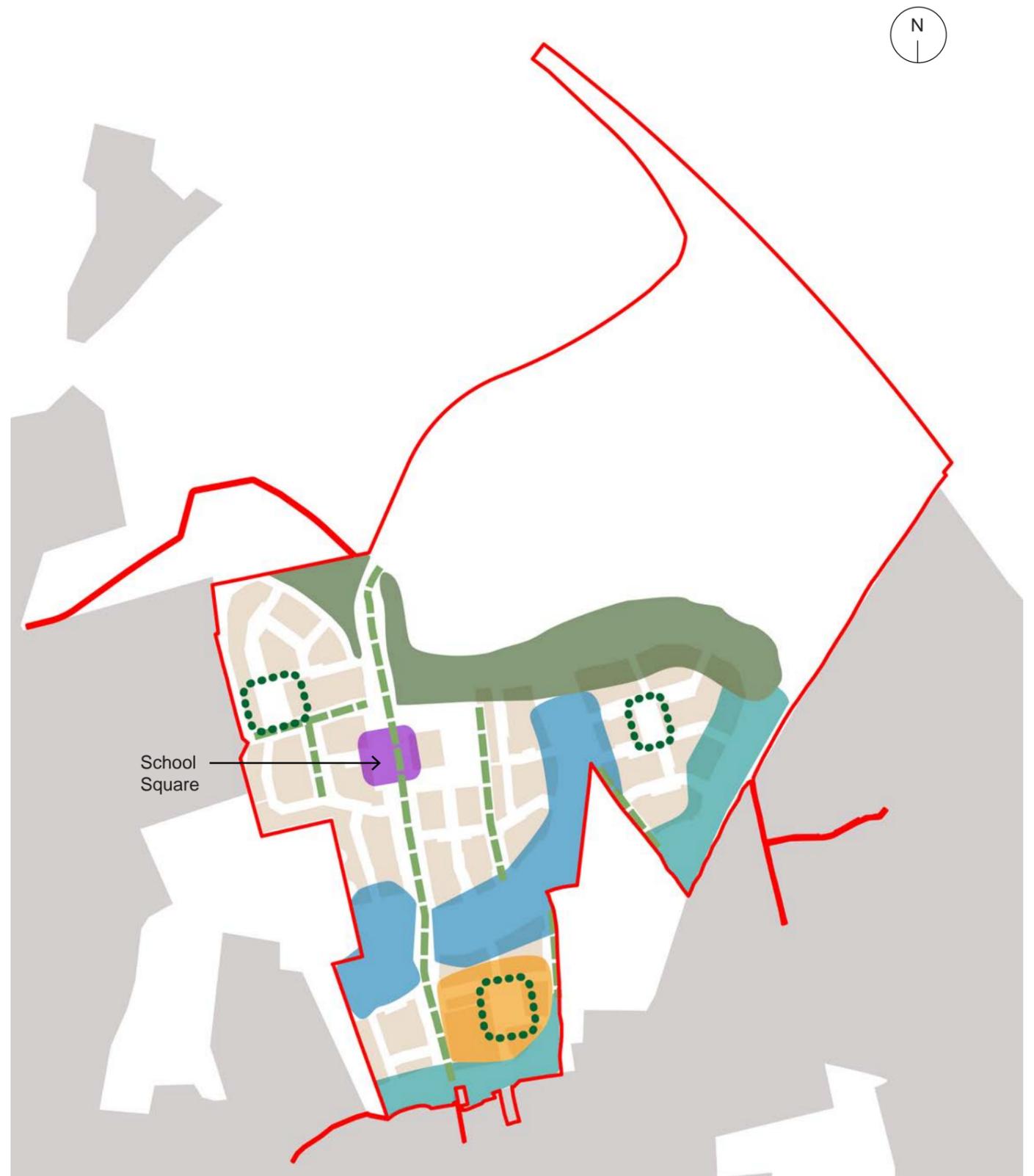
3.2.5 Character

The landscape principles illustrated on page 31 have provided a framework of the character of the whole neighbourhood.

The range of landscape settings together with the pedestrian network has provided the pattern of streets and houses. The composition of landscape and development together creates a range of neighbourhoods that is easy to find your way around. Important components of this framework include providing a sense of arrival, the creation of legible routes to the local and neighbourhood centres and streets that open out on to green spaces and hedgerows.

Each neighbourhood has its own unique character based on the surrounding landscape setting. Further explanation of these character areas can be found in Section 6.4.

- Assessment boundary
- Ivanhoe Meadows
- Brook Lanes
- Money Hill Park Fringe
- School Square
- Southern neighbourhood
- Residential Greens
- The Hedgerows



Character Principle Plan

4. SUSTAINABLE DRAINAGE

4.1 DRAINAGE RATIONALE | 4.2 SuDS PRINCIPLES | 4.3 SUSTAINABLE DRAINAGE COMPONENTS | 4.4 DRAINAGE STRATEGY

4.1 Drainage Rationale

The drainage rationale proposed for the Money Hill development integrates a Sustainable Drainage System (SuDS) approach to managing surface water. This is intended to reduce the potential risk of flooding, improve water quality, create amenity spaces and improve biodiversity.

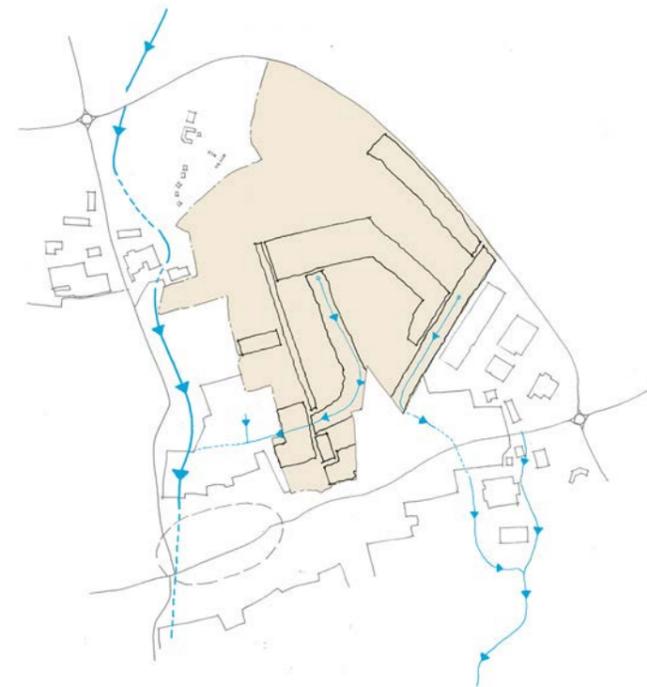
A holistic approach to sustainable drainage has been taken with SuDS components integrated into the 'soft' landscape and 'hard' public realm areas.

This approach has influenced the design of the proposed Masterplan, particularly the location of development away from low lying ground adjacent to the watercourses. The following diagrams demonstrate the approach taken to the design development of the sustainable drainage for the site.



4.1.1 Existing Topography, Watercourses and Vegetation

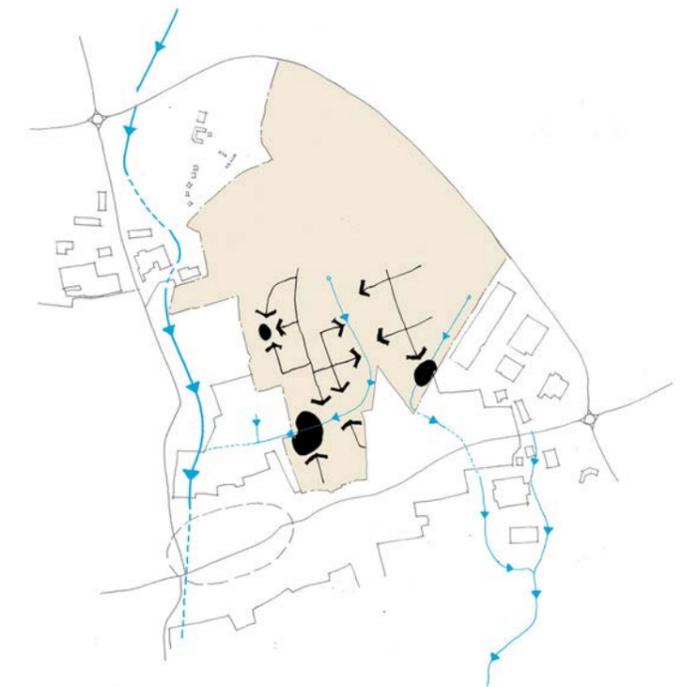
The existing site topography, watercourses, tree groupings and hedgerow network mapped together, showing that they often occur in the same locations and will influence the drainage strategy. This has significantly influenced the structure of the proposed drainage strategy.



4.1.2 Integrating the landscape framework with the existing watercourses

The landscape framework is made up of a number of linked open spaces that follow the existing watercourses and hedgerow network. The SuDS components of the Masterplan are integrated into this framework, resulting in a series of multifunctional open spaces.

- The spaces that are created through this approach have a number of different functions, including:-
- Providing sustainable drainage;
 - Creating wildlife habitats and corridors;
 - Improving site biodiversity;
 - Creating new amenity spaces for the local community to enjoy; and
 - Providing a local educational nature resource.



4.1.3 Integrating the SuDs into the Proposed Street Network

The proposed landscape framework and the topography of the site have both significantly influenced the design of the proposed street network. Most of the streets are aligned to follow the existing land contours. This alignment complements the sustainable approach proposed for the management of surface water as it incorporates gravity falls into attenuation areas across the site.

4.2 SuDS principles

National, regional and local planning policy and guidance support a SuDS approach to creating sustainable development. For further details please refer to the supporting Drainage Strategy Report produced by Buro Happold.

The key principles of SuDS are to:-

- Manage surface water runoff rates, reducing the impact of localised flooding;
- Protect water quality; and
- Enhance local wildlife habitats and site biodiversity

The benefits of SuDS are:-

- The effective control of peak flows;
- Improved water quality;
- Reduced surface erosion; and
- Water conservation through rainwater harvesting.

4.3 Sustainable Drainage Components

There are a number of drainage components within a SuDS approach that manage the surface water in different ways. Those that are proposed for the Money Hill development are outlined below. Most of the components are landscape based, but there are also engineered solutions proposed for particular circumstances.

4.3.1 Wetland Areas

A wetland area will occur along the watercourse corridor. The existing watercourse channel will be re-profiled to provide online storage for storm water. This will improve the biodiversity of the watercourse and will create a new amenity space with reed and marginal planting.

4.3.2 Attenuation Areas

Attenuation areas will be introduced near to the points where the existing watercourses exit the site. These ponds will form the final sections of the SuDS management train, controlling the release of water into watercourses beyond the site. Most of the time they will be semi dry basins, but they will have the capacity to attenuate peak storm run off flow. There is an opportunity for them to have grassed terraced banks and areas of reed, tree and marginal planting which will improve biodiversity and provide a new amenity space for local residents.

4.3.3 Swales and Basins

The principle of a swale network is to collect and move water through a series of interconnected shallow grassed or vegetated channels. At Money Hill, swales and grass basins are proposed to run along the watercourse corridor and in the open spaces near the residential blocks.

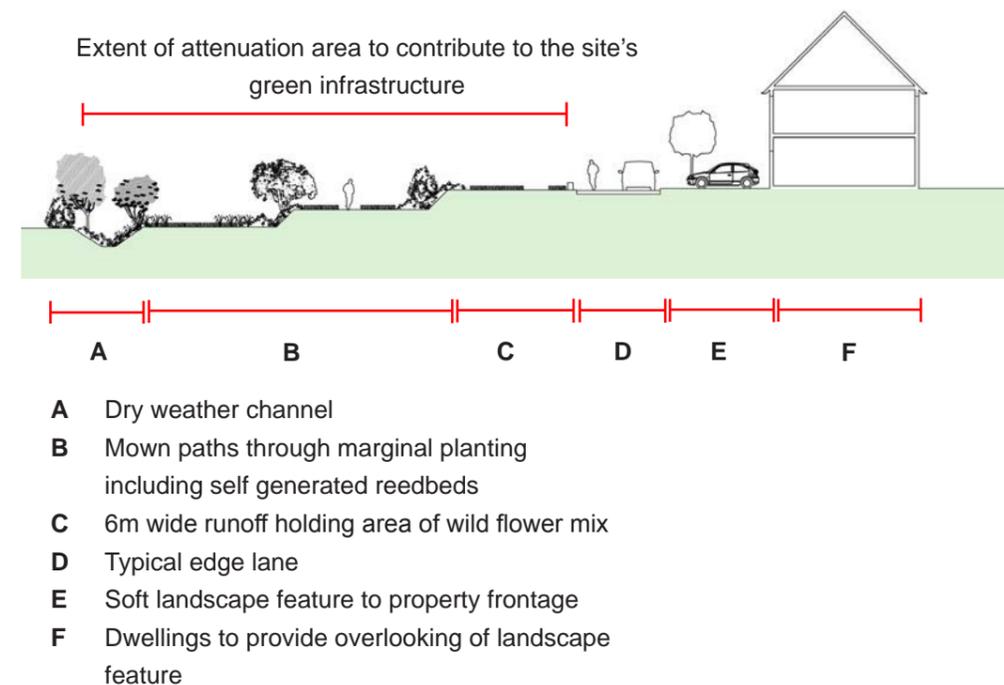


Swale with grassed banks and new tree planting



Example of an attenuation area with attenuating a peak storm flow

SuDS Principle Diagram



4.3.4 Permeable Paving

The Drainage Strategy Report recommends that a permeable paving system should be implemented at the local centre area proposed within the Money Hill development. This area will have a greater surface area of paving than any other and would benefit from a sustainable solution for collecting surface runoff water and conveying it to a nearby attenuation area.

4.3.5 Green Roofs

Green roof systems are designed to intercept and retain rain water, helping to reduce the volume of run off by attenuating peak flows. The National Forest Company supports the incorporation of green roofs and walls to aid 'forest identity' and to promote sustainable design within its designated forest area. It is therefore proposed that green roofs / sedum roofs be incorporated in the design of the potential community hall, the school building and on some small secondary structures.

4.3.6 Rainwater Harvesting Systems and Grey Water Recycling

Collecting rain water from roofs and hard surfaces can help to intercept run off and conserve water. It is recommended that all buildings at Money Hill have rainwater butts to collect and store water for domestic and garden use. Water butts will also need to be located within allotment areas to collect water for use by local residents.

4.3.7 New Tree Planting and Landscaping

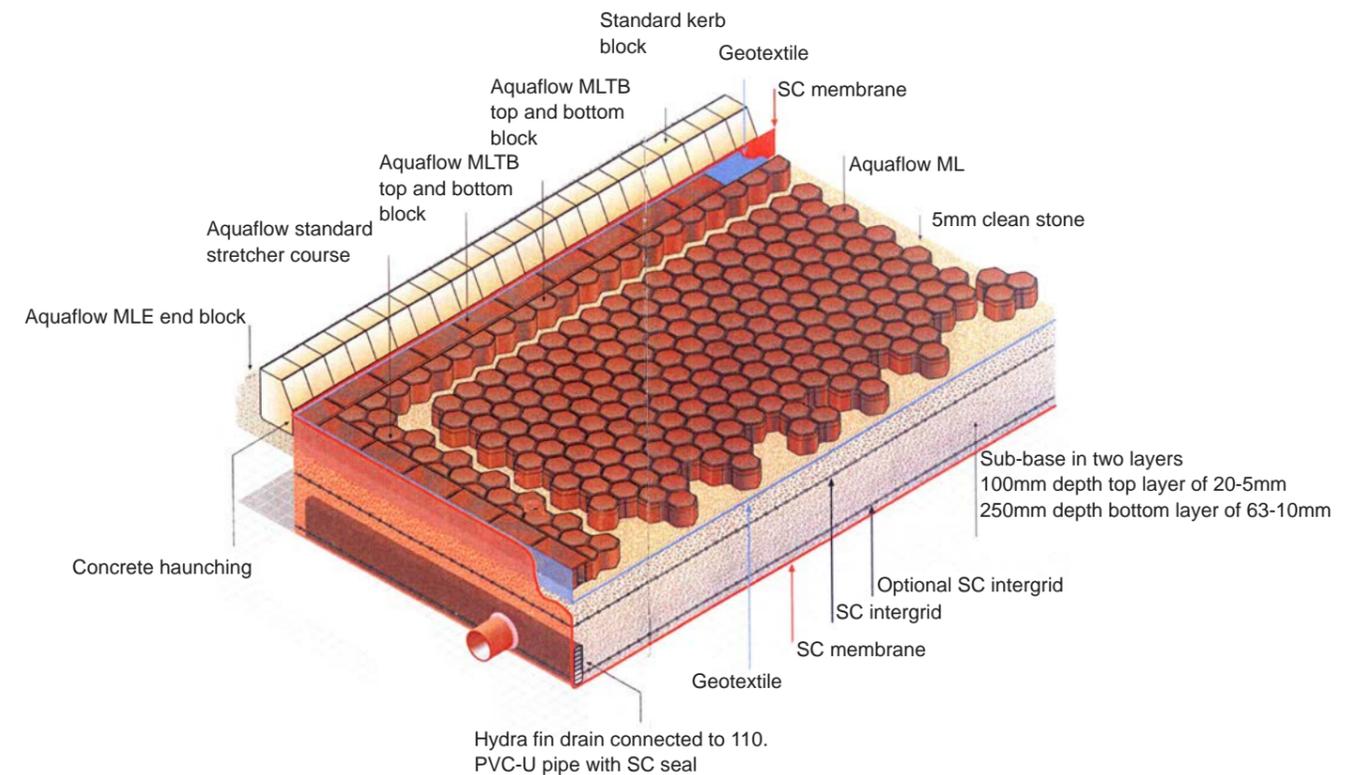
In line with the National Forest Company's aspirations, new tree planting and landscaping will help to intercept rain water, thus slowing down surface water run off rates whilst also creating high quality amenity spaces and habitats for local wildlife.



Contemporary building with an integrated green roof



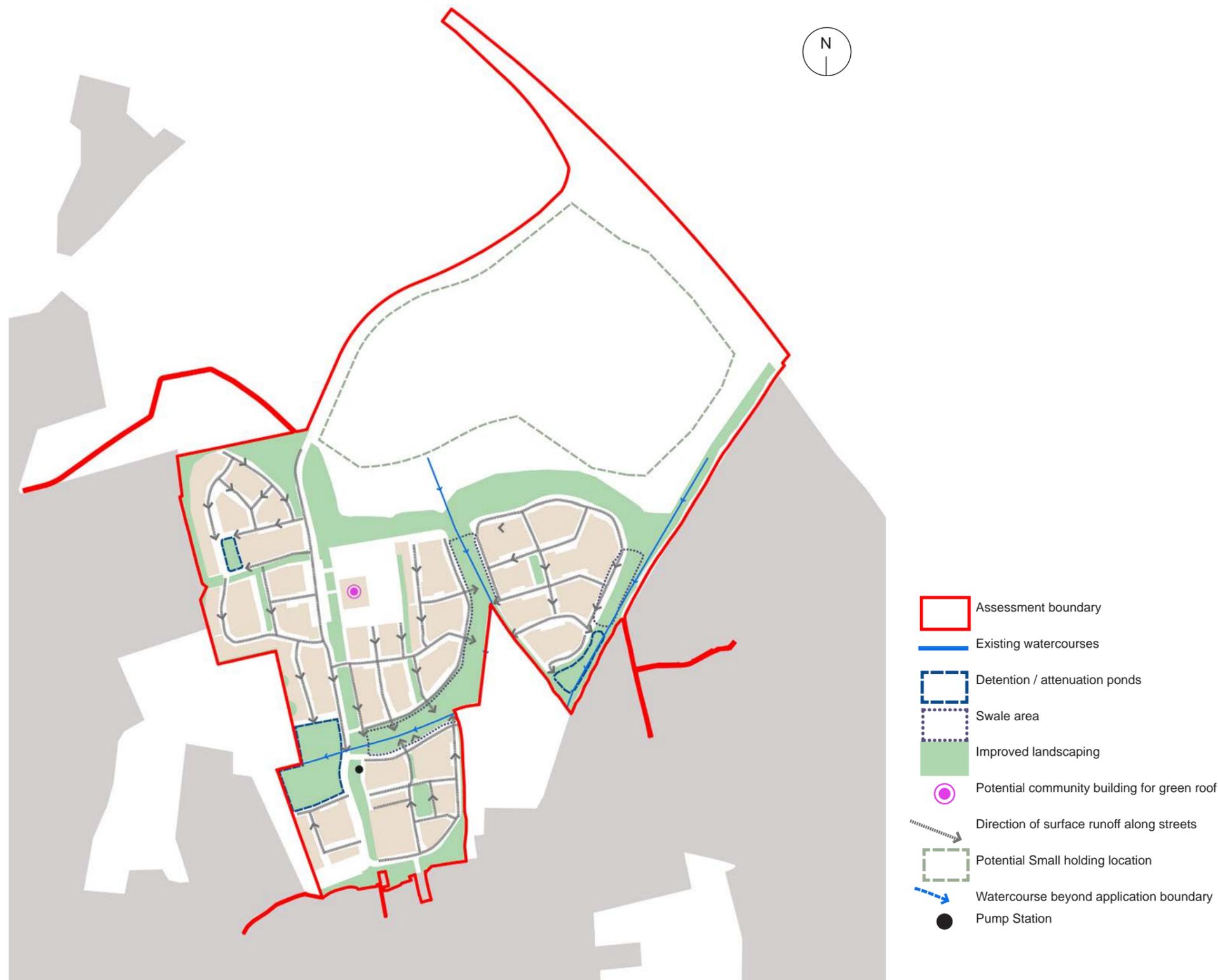
Example of a shed with integrated grey water harvesting system



Typical composition of porous paving system

4.4 Drainage Strategy

The plan shows how the SuDS components will be applied across the site to create a comprehensive Drainage Strategy for Money Hill.



Drainage Strategy Plan

5. ACCESS AND MOVEMENT

5.1 Access and Movement Rationale

The access and movement rationale behind the Masterplan proposed for Money Hill creates a well connected neighbourhood that is easy to navigate, has well defined pedestrian and cycle routes, offers quick and easy access to the existing town centre, accommodates a local bus service and features well designed streets with appropriate car parking that does not detract from the street scene.

The movement network is based on guidance from Manual for Streets (2007), Manual for Streets 2 (2010) and Leicestershire County Council 6 C's Design Guide. Amongst other design influences, these guidance documents aim to achieve ease of access and low traffic speeds.

The rationale a incorporates the following key themes.

5.1.1 A Connected Place

- Well-designed connections between the new neighbourhood and the existing town centre will be introduced in order to ensure good public transport links with new and existing facilities.
- Pedestrian links along Ivanhoe Way and other links beyond the site will be improved.

5.1.2 A Walkable Neighbourhood

- The potential medical centre will be located close to the town centre so that it is easily accessible for both new and existing residents of Ashby (please refer 3.2.1 Use).
- To help encourage a healthy lifestyle, a network of leisure routes will be introduced, providing access to local open space amenities, creating attractive circular walks around the area beyond the site and connecting the development into the National Trail.
- Safe pedestrian and cyclist routes will be provided to the school, the play areas and other community facilities in order to encourage walking and cycling.

5.1.3 A Sustainable Transportation Network

- A new bus service will be provided to connect the new neighbourhood to Ashby town centre.
- Bus stops will be located at key destinations such as the potential medical centre, the school and the local centre area so that all residents can easily access bus services.
- The movement hierarchy will be legible.

5.1.4 A Legible Street Network

- The design of the movement network has been informed by a well-defined street hierarchy in order to ensure that it is easy to distinguish between the main movement arteries, the residential streets and the more localised community links.
- The street hierarchy will assist with way finding and will help to reduce car speeds in the residential areas, thus creating a safer environment for pedestrians.

5.1.5 A Sense of Place

- A well-planned and carefully designed movement network can greatly contribute to the local distinctiveness of a place.
- Money Hill will have a distinctive identity with well-organised community spaces, safe residential streets and quite edge lanes adjacent to open spaces.

5.1.6 Access and Planning Policy

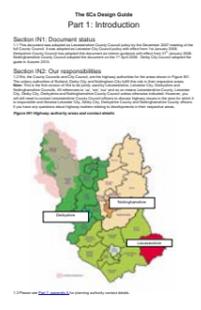
The access rationale has been prepared in consultation with Leicestershire County Council and bus operators, who seeks to reduce congestion, and the need for private car. The Money Hill Masterplan has achieved this by creating a new bus link that will pass into the development area.

In accordance with the Paragraph 50 of the National Planning Policy Framework the scheme plans for a mix of housing based on current and future demographic trends, market trends and the needs of different groups in the community (such as families with children, older people, people with disabilities). In particular, the Masterplan, and its internal access, has had particular regards to older people and people with disabilities by positioning extra care housing adjacent to the main bus route that passes through the site. Potential extra care housing has also been positioned alongside potential health care facilities to minimise distance to travel. This positive design has led to East Midlands Housing Association, as a potential operator of the extra care housing concluding:

“With regard to your proposed extra care at Money Hill in my opinion there are some very strong attributes. The project’s proposed location, set within a larger residential development, close to the town centre and also the nearby “relocated” health centre, is very positive”
East Midlands Housing Association.



Manual for Streets and Manual for Streets 2



Leicestershire County Council's 6Cs Design Guide

5.2.1 Vehicular Movement and Access

There are two vehicular access points proposed into the site; the northern access from the A511 and the southern access from Nottingham Road. The new A511 bypass will involve the construction of a new roundabout. The new access point at Nottingham Road will involve the occupation of two house plots and the layout of a new road.

To limit the amount of traffic that the new access point could create at Nottingham Road, southern vehicular movement will be restricted. See pages 45 and 46 for more information.

The development proposals will create improved pedestrian / cycle linkages. These include improved linkages to the town centre, local schools, recreation routes and surrounding neighbourhoods. A new alignment of Ivanhoe way is proposed to improve the quality of the footpath and its landscape setting. The original alignment will remain in situ. A new pedestrian linkage will be created to the north west of the application site, which will pass alongside Moneyhill Farm and provide access onto Smisby Road, which is the main north/south thoroughfare in the town. These new pedestrian routes will also provide improved access to Woodcote Primary School.



Vehicular Movement and Access Plan

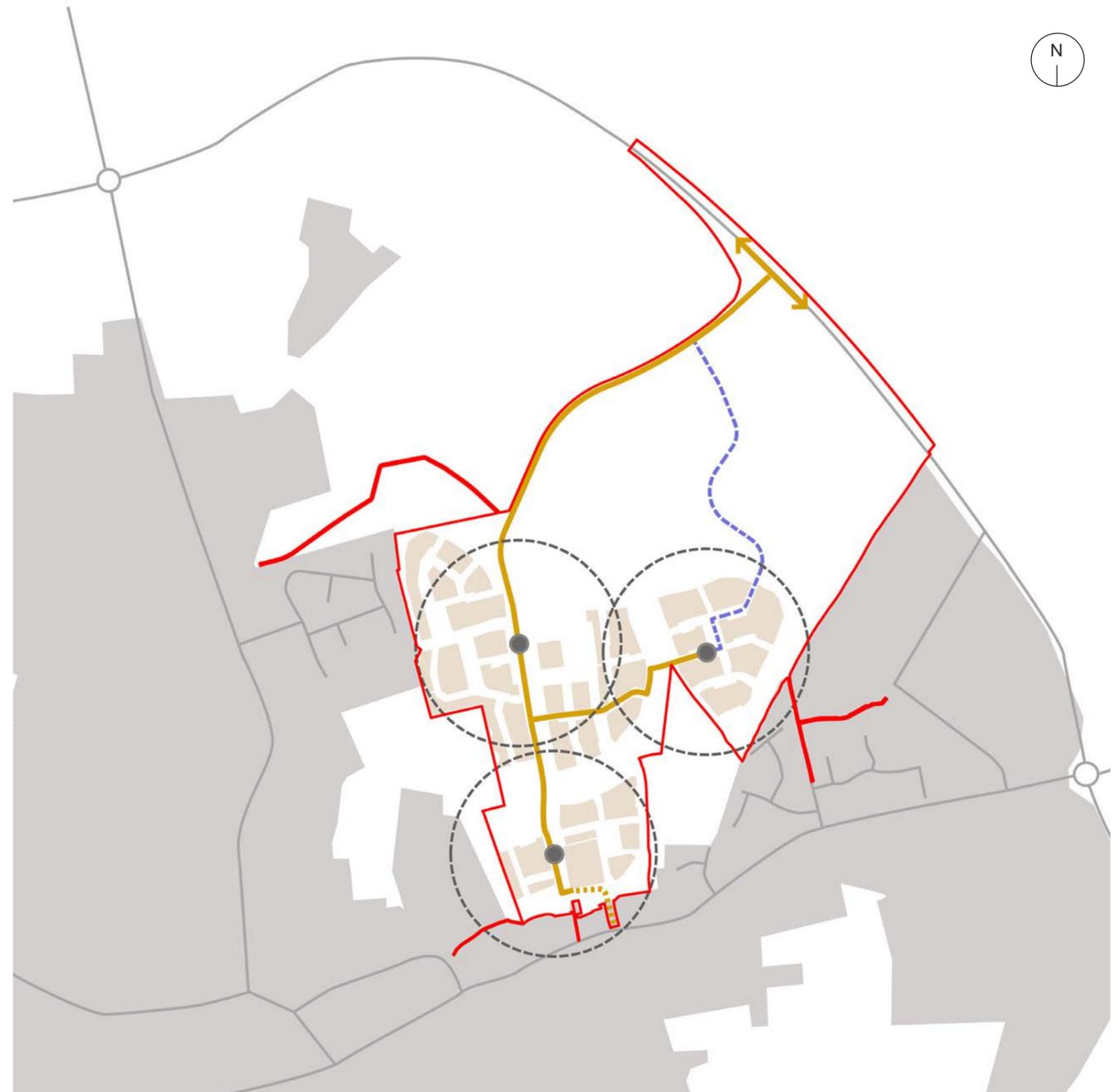
- | | | | |
|---|-----------------------|---|---|
|  | Assessment boundary |  | Pedestrian & cycle only access subject to detailed design |
|  | North access point |  | Proposed development |
|  | South access point |  | Existing urban form |
|  | Vehicular circulation |  | Existing road network |
|  | Bus gate | | |

5.2.2 The Bus Route

To offer an alternative to using private cars, a new bus route is proposed to serve both the southern and northern phases of Money Hill. There will be a number of bus stops strategically located throughout the development, ensuring that most residents are within a 5-minute walk from a stop. The bus service will provide a potential new route for Ashby that will connect the town centre with the new neighbourhood.

See pages 45 and 46 for more information on the southern area.

-  Assessment boundary
-  Bus stop and 400m walking distance
-  Proposed bus route
-  Potential future bus route
-  Proposed development
-  Existing urban form
-  Existing road network



Bus Route Plan

5.2.3 Pedestrian and Cycle Movement

There are eight access points proposed to connect pedestrians and cyclists the new development area into Ashby. They include:

Locations 1 and 2
These are located in the south west corner of the site and connect the new development area to Town Centre via North Street and Wood Street. These will also serve the new town centre car parking.

Location 3
This is also located to the southern corner of the site and connects the new development area to Town Centre via Wood Street and Nottingham Road. It is a pedestrian and cycle access point alongside the new highway link.

Location 4 and 5
These are located to the south east of the site and link Ivanhoe Way at the boundary of the site to ensure continuity of route is possible across third party land.

Locations 6
This is located to the east of the development area and connect the new housing to Featherbed Lane and Plantagenet Way. The close proximity of the Tesco's food store and Nottingham Road bus stops will provide residents with good services within a short walk/ cycle through good quality landscape.

Location 7
This pedestrian and cycle access point is located on Ivanhoe Way close to the A511 underpass. By enhancing the existing route as part of the National Trail, development at Money Hill has the potential to enhance a series of green corridors linking Ashby to the wider countryside beyond the A511.

Location 8
This is located to the northwest of the development area and connects the new housing to Smisby Road. It will provide good pedestrian and cycle access to the Smisby Road employment area and bus services. It is also anticipates that an informal footpath link will allow leisure walks north following the line of the existing footpath O80 passed Cliffthorpe.

These connections provide a great opportunity for the new neighbourhood including cycle routes. The majority of these cycle routes will be off the highway and are characterised as 'greenways', such as along the central double hedgerow, through the park and along Ivanhoe Way.

-  Assessment boundary
-  Pedestrian and cycle access points
-  New footways and leisure routes
-  New Cycle routes (mostly car free routes)
-  Existing footpaths
-  Existing cycleway
-  Proposed development
-  Existing urban form
-  Existing road network



Pedestrian and Cycle Movement Plan

Connections 1 - 4, the North Street- Nottingham Road area

As mentioned earlier, a new pedestrian, cycle and vehicular access point will be created at Nottingham Road.

It will allow access for:

- Bus access in both directions;
- Cycle and pedestrian access in both directions;
- Access to a **limited** number of dwellings in both directions; and,
- Access to car park, but not egress as this will be directed north to the A511.

To facilitate this constrained movement network and to function well, a number of devices will be used such as a bus gate and car park controls.

These two sketches illustrate the access principles to support the constrained movement network through the southern neighbourhood.

Bus route

A new bus route will run through the new access point at Nottingham Road.

A bus gate will be located close to the car park to allow for buses and emergency vehicles only to egress to Nottingham Road. It will prevent other vehicles from egressing to Nottingham Road.

The physical bus gate will be in the form of a rise and fall bollard. To function as intended, this feature will be integrated into the car park access arrangement subject to further detail.



Example of a bus gate with rising bollards

-  Public bus service route- potentially in each direction.
-  Bus gate to prevent eastward flow of cars.



Bus route principle

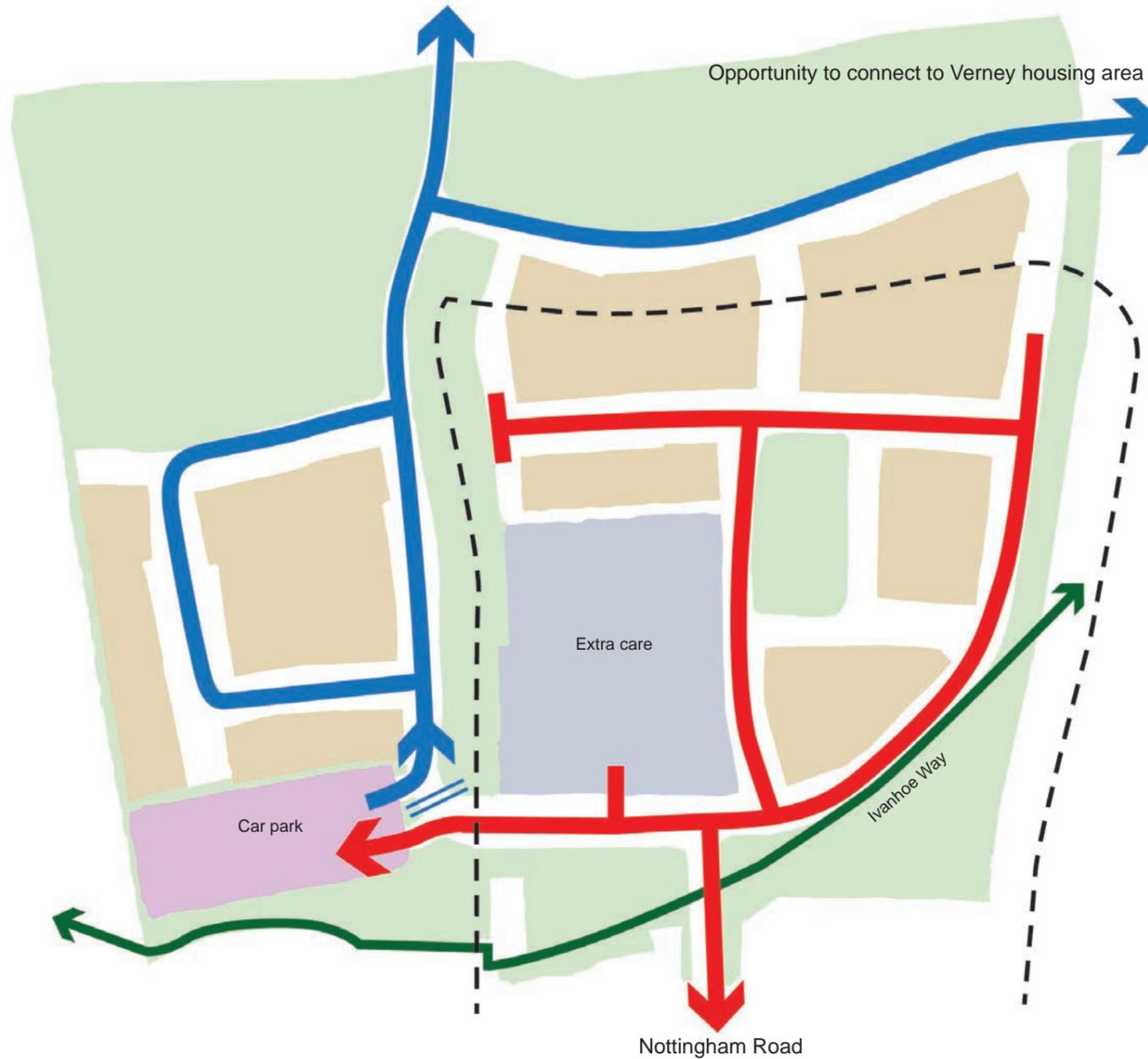
Street network

The new access point will provide vehicular access and egress to: the potential extra care, a number of houses and the car park. Egress from the car park will be via A511. To ensure that cars only egress north from the car park, the out gate will be on the north side of the bus gate.

- █ Access & egress from Nottingham Road:
 - Limited houses
 - Potential Extra Care

█ Main streets of the northern movement network

Extend of catchment that allows car egress to Nottingham Road

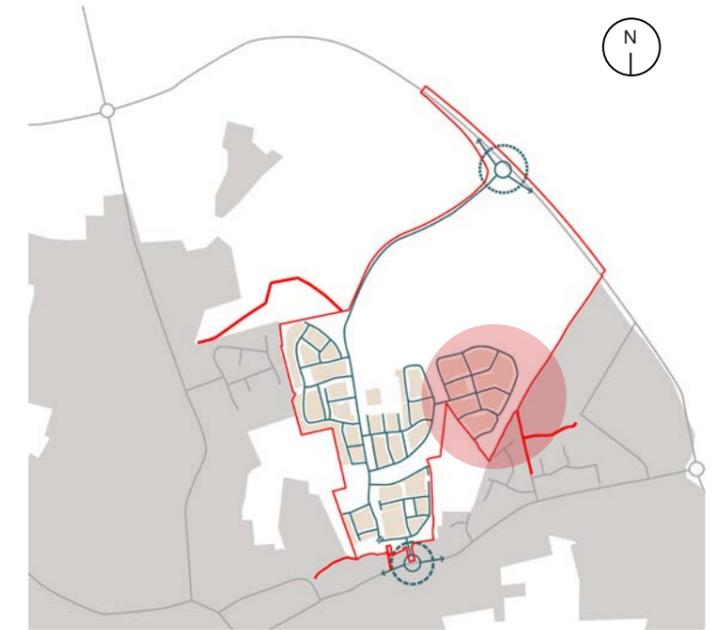


Street network principle



Location of South corner connections

Connections 5 - 6, the Featherbed Lane area



Location of Featherbed Lane area

This illustrative plan is only intended to suggest the form desired for connections in the Featherbed Lane area. Specific designs will form part of later proposals



View of new open space and houses from Featherbed lane

This sketch perspective is only intended to suggest the form desired for connections in the Featherbed Lane area. Specific designs will form part of later proposals

Connection 8, the Smisby Road Approach



Location of Smisby Road Approach

This illustrative plan is only intended to suggest the form desired for connections in the Smisby Road area. Specific designs will form part of later proposals



Facing East



Facing East



Facing West



B



C



D



View of new housing from proposed Smisby Road pedestrian/cycling connection

This sketch perspective is only intended to suggest the form desired for connections in the Smisby Road area. Specific designs will form part of later proposals

5.2.4 Inclusive Design Principles

The Money Hill Consortium is committed to ensuring that the Money Hill development meets the highest standards of access and is inclusive of all, regardless of ability, age or gender. As a result, the development has been designed to be as accessible as possible, taking into account the current government legislation and guidance set out in the documents listed below:

- Guidance on information requirements and validation by CLG (March 2010);
- CABE's Design and Access Statements - how to write, read and use them;
- Part M of the Building Regulations and Approved Document M, 2006 edition;
- Lifetime Homes Standards;
- Access by Design, the Journal of the Centre for Accessible Environments;
- Access for Disabled People: Practice Advice Note No. 3 by the Royal Town Planning Institute (1988);
- The Disability Discrimination Act 1995;
- Access Journal (the Journal of the Access Association);
- Accessible Thresholds in New Housing: guidance for house builders and designers, DETR (The Stationery Office, 1999);
- Barrier-free Design - A manual for building designers and managers, James Holmes-Siedle (Butterworth Architecture, Oxford, 1996);
- British Standard BS8300 2001 (Design of Buildings and their approaches to meet the needs of disabled people);
- British Standard BS5588 2004 Part 8 (Code of practice for means of escape for disabled people);
- British Standard BS5588 2004 Part 12 (Code of practice for managing fire safely).

Taken together, these standards and guidelines improve the quality of inclusive design beyond the minimum requirements. However, it is Part M of the Building Regulations and the requirements of the Disability Discrimination Act that set out the mandatory requirements that designers must comply with. All subsequent participating developers will be required to meet these specific obligations.

The most relevant clauses contained within Part M of the Building Regulations include the following:

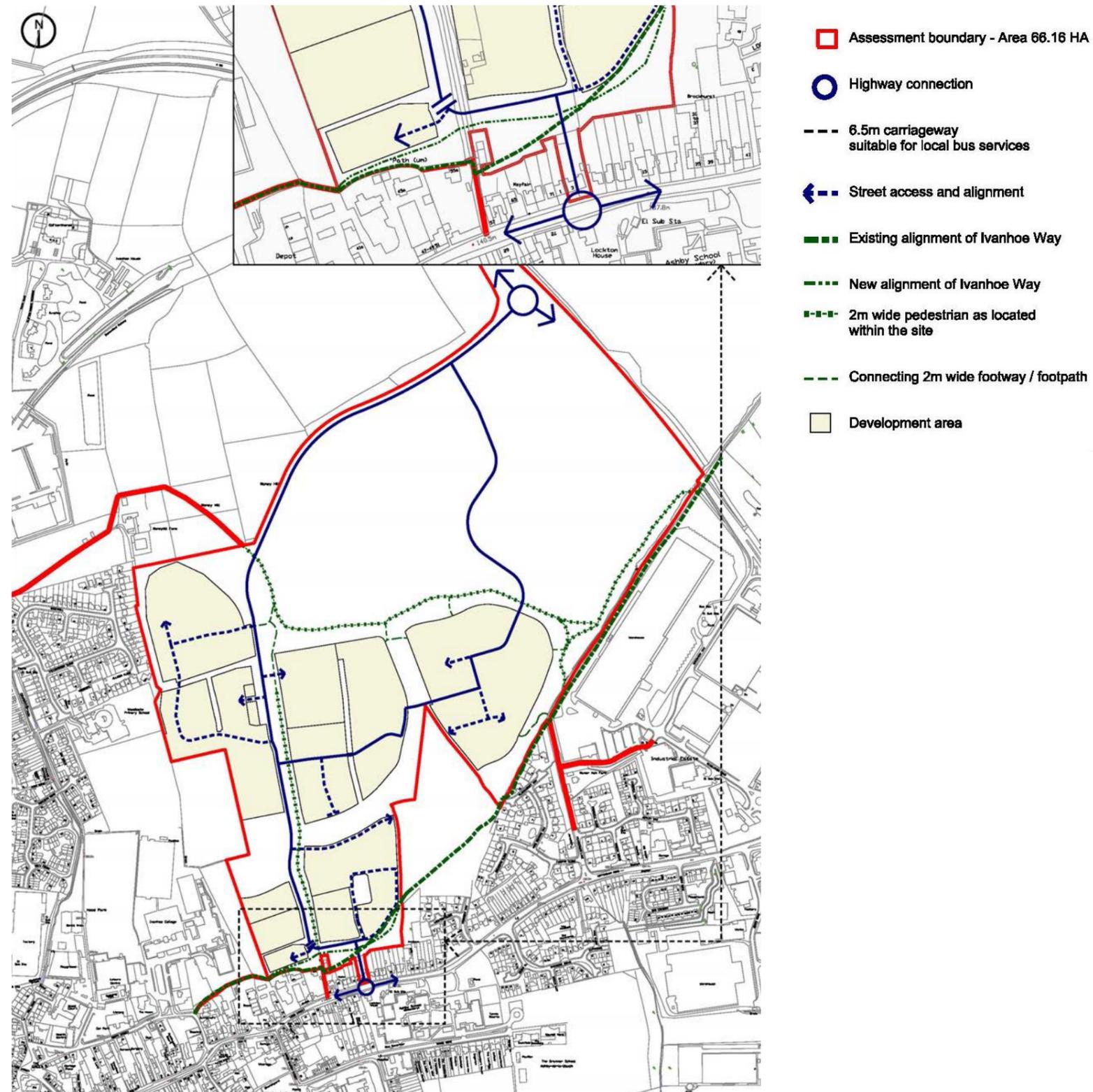
6.5 Normally, the provisions will apply to the approach to the principal entrance. [The principal entrance is that which a visitor, not familiar with the building, would normally expect to approach]. However, if that is not possible in a particular situation, it would be reasonable to apply them to a suitable alternative entrance; and

6.6 The approach should be as safe and as convenient for disabled people as reasonable, and, ideally, be level or ramped. However, on steeply sloping plots, a stepped approach would be reasonable.

Under the Disability Discrimination Act 1995 it is unlawful for persons who provide services to members of the public to discriminate against disabled people by treating them less favourably for a reason related to their disability or by failing to comply with a duty to provide reasonable adjustments.

5.3 The Movement Framework

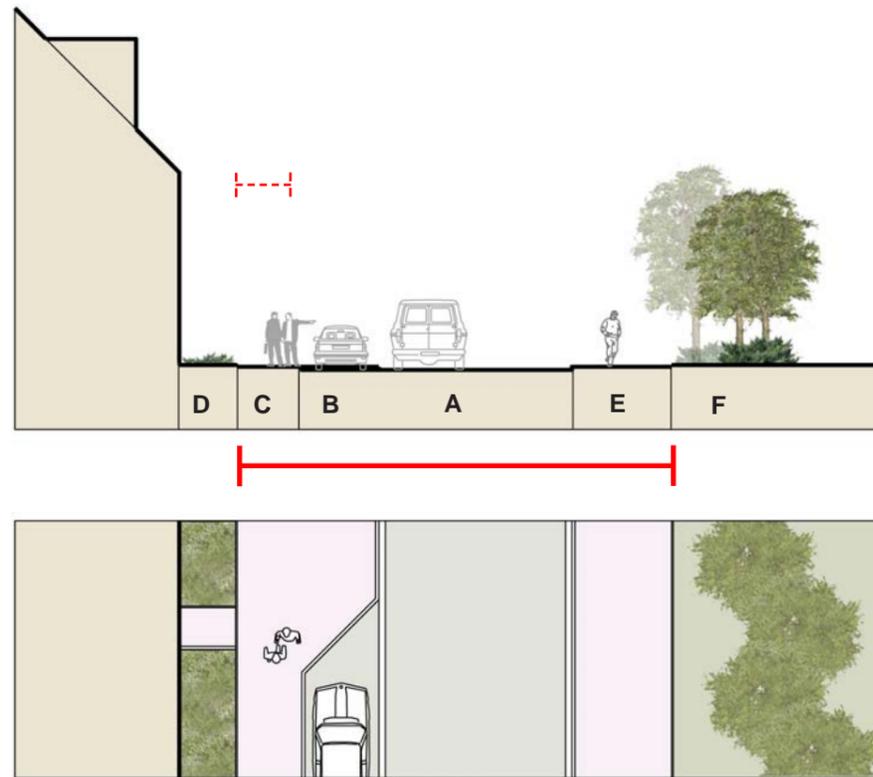
The adjacent plan shows the accumulative movement framework that is proposed for the site, illustrating the integration of pedestrian, cycle, bus and car networks. The streets are laid out as a grid to ensure good permeability throughout the new neighbourhood. The hierarchy of routes that is proposed will help users and visitors to navigate around the development.



Movement Parameter Plan

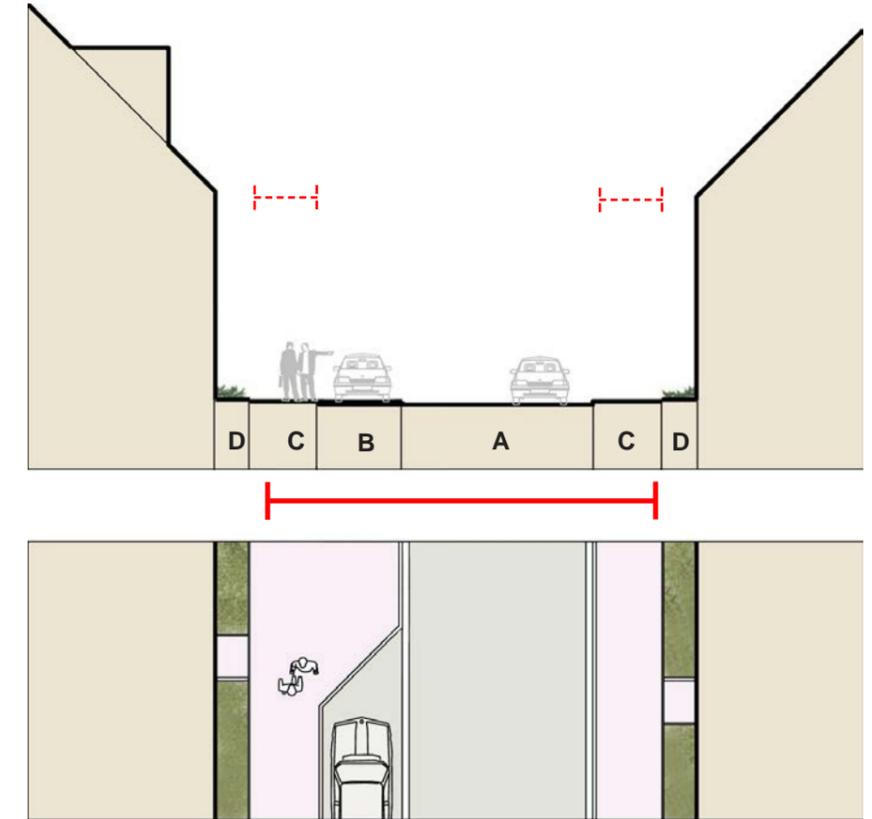
5.3.1 Street Hierarchy

The following diagrams show the different street characters across the site in the form of a plan and cross section. These street characters reflect the movement hierarchy and how streets will respond to location of uses and landscape features.



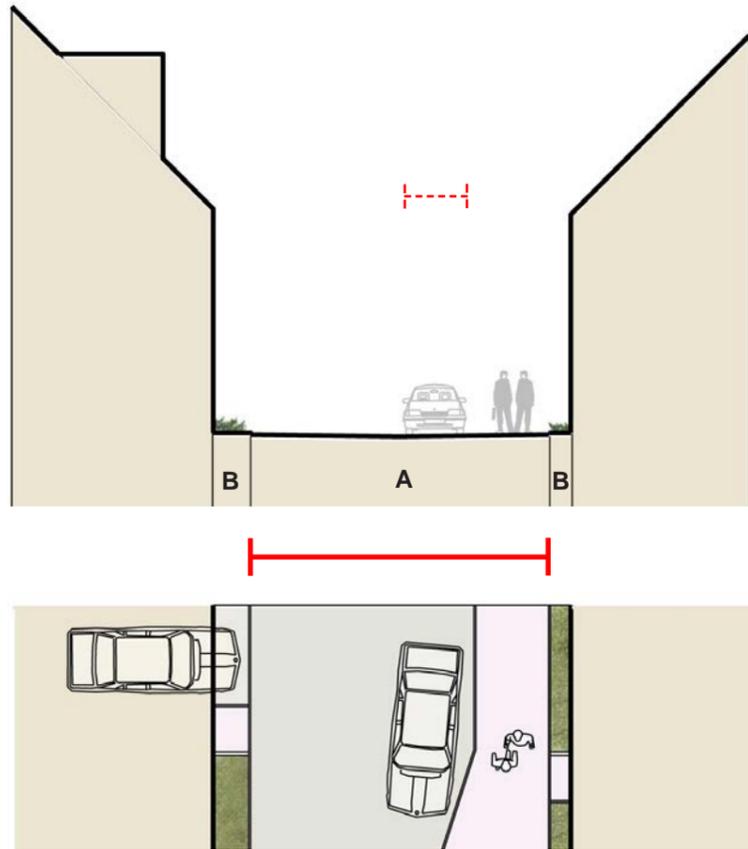
Bus Route

- A 6.5m min. carriageway
- B 2.5m on-street parking in selected locations
- C 2m min. footway
- D 2m - 7m private set-back
- E 3m pedestrian / cycleway
- F Open space / hedgerow corridor
- Extent of adoptable area
- - - No less than 2m wide service strip



Typical Residential Street

- A 5.5m min. carriageway
- B 2.5m on-street parking in selected locations
- C 2m min. footway
- D 1m - 6m private set-back
- Extent of adoptable area
- - - No less than 2m wide service strip



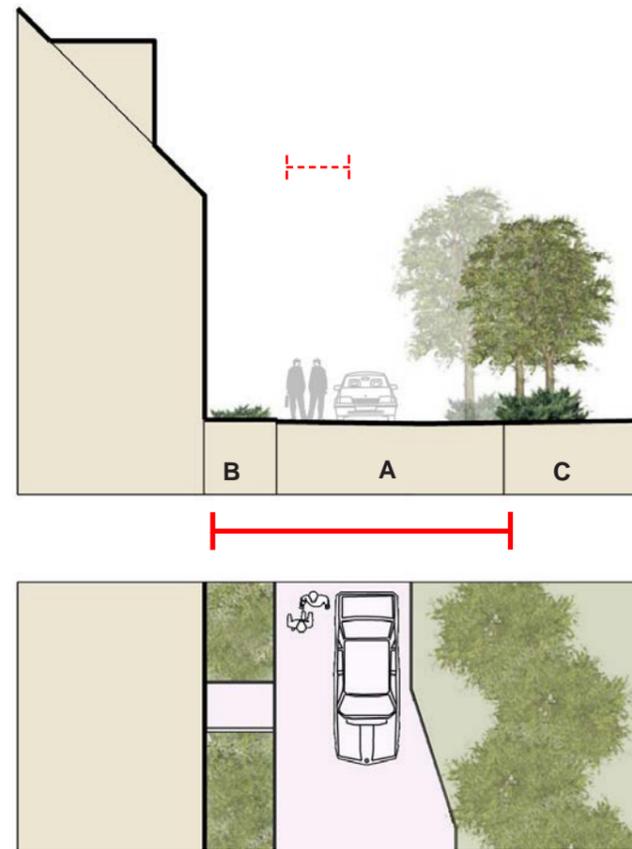
Typical Shared Surface Mews Street

A 4.8m - 6m min. subject to access arrangements

B 0.6m - 1.2m on-street parking

Extent of adoptable area

No less than 2m wide service strip



Typical Edge Lanes

A 3.5m - 6m min. subject to access arrangements

B 1m - 6m private set-back

C Hedgerow / open space

Extent of adoptable area

No less than 2m wide service strip

5.4 The Parking Strategy

The parking strategy for Money Hill has been developed as an integral part of the design process in order to ensure that sufficient car parking is provided, that it is conveniently located and that it does not detract from the overall aesthetic appeal of the streetscape.

There is a variety of parking arrangements that can be used to provide sufficient car parking for both residents and visitors. Generally however, these can be divided into three predominant categories; on-plot, on-street and off-plot (Car Parking: What Works Where by English Partnerships, 2006).

The Masterplan proposals include the potential for off-plot parking in rear courts. If this arrangement is used, special care will need to be taken with regard to the size, accessibility, overlooking and appearance of the courts. Following the 'Building for Life 12' guidance (Building for Life Partnership, 2012), parking courts must serve no more than five properties, must be well overlooked by at least one house frontage and must have a structured landscape treatment. Indeed, whichever parking solution is selected, an integrated approach to landscaping will be required as this is fundamental to the design of any street.

In order to ensure parking is secure and well surveyed, it is important for it to be closely related to the dwelling it serves. For more detail, please refer to 'Safer Places: The Planning System and Crime Prevention' (2004), the main attributes of which are listed below:

- Access and Movement: places should have well defined routes, spaces and entrances that provide for convenient movement without compromising security;
- Structure: places should be structured so that different uses do not cause conflict with each other;
- Surveillance: all publicly accessible spaces should be overlooked;
- Ownership: places should promote a sense of ownership, respect, territorial responsibility and community;
- Physical Protection: places should include any necessary security features in a well-designed manner;
- Activity: the level of human activity should be appropriate to the location and should result in a reduced risk of crime and a sense of safety at all times; and
- Management and Maintenance: places should be designed with management and maintenance in mind in order to discourage crime in the present and in the future.

5.4.1 Car Parking Dimensions

All car parking spaces provided within the Money Hill development should adhere to the following dimensions:

- 2m x 6m when cars are parked in line with the pavement;
- 2.5m x 5m when cars are parked at a right angle to the pavement;
- 2.75m x 5m when spaces are enclosed on one side; and
- A minimum of 3m x 5m when spaces are enclosed on two sides.

5.4.2 Garages and Car Ports

Any garages or car ports provided within the Money Hill development should be a minimum of 3m wide by 6m deep for a single car or 6m wide by 6m deep for two cars. Internal dimensions must provide enough space to accommodate a cycle as well as supplementary storage areas for items such as refuse bins.

In order to preserve the aesthetic appeal of the street scene and ensure that natural surveillance is not compromised, all garages must be set back from street either in line with the building or to the rear of the plot.

5.4.3 Parking Examples

The following diagrams (overleaf) illustrate the parking arrangement options. They are accompanied by an extract from the illustrative Masterplan for Money Hill, which demonstrates how they could all work together to ensure that the objectives of the Parking Strategy are met within a particular part of the new neighbourhood.

Money Hill will adhere to Leicestershire County Council guidance on residential off street parking provision which states that:-

- Dwellings with 3 bedrooms or fewer require a minimum of 2 parking spaces;
- Dwellings with 3 or more bedrooms require a minimum of 3 parking spaces; and
- In Local Authority or Housing Association developments, flats with two bedrooms or fewer require 3 spaces for every 2 dwellings.

1. On Plot Parking

In most cases where parking is provided on building plots, it will be located to the sides of the dwellings. In many cases it will be provided in integral garages and garages set back from street edge.

2. In Line, On Street Parking

In this arrangement, parking is provided along the kerbside, parallel to the footway. It could be provided in marked or unmarked bays and has the potential to accommodate unallocated visitor parking. In order to preserve an attractive street scene, an integrated landscape treatment is vital.

3. 90 Degree Parking on a Private Frontage

In this arrangement, kerbside parking is provided at a right angle to the footway. The parking is set on a private frontage behind the adoptable footway. To ensure accessibility, a footway of at least 1m in width will be required between groups of parked cars. This arrangement also requires an integrated landscape treatment to ensure that the parked cars do not dominate the street scene.

4. Grouped On Street Parking

Where parking is arranged in groups on the street, shared surface areas with subtle parking space markings will be provided. This arrangement is only suitable for small groups of cars and requires an integrated landscape treatment.

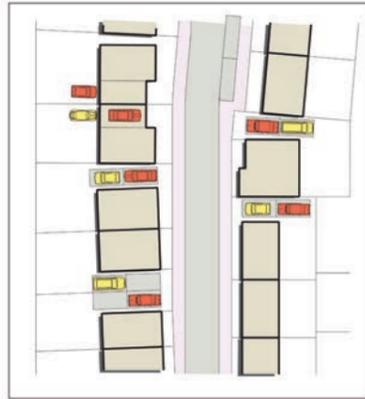
5. Small Rear Parking Courts

Where small rear parking courts are used, they will serve a maximum of five properties. In order to ensure safety and reduce crime, at least one house frontage must overlook each court. In order to ensure an attractive outlook, an integrated landscape treatment will be required.

6. Front of Plot Parking

In some cases, on plot parking will be provided at the front of the plot. In these cases cars will be parked at a right angle to the footway and an integrated landscape treatment will be required.

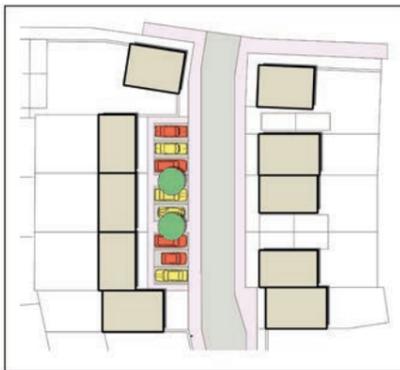
1. On Plot Parking



2. In Line, On Street Parking



3. 90 Degree Parking on a Private Frontage



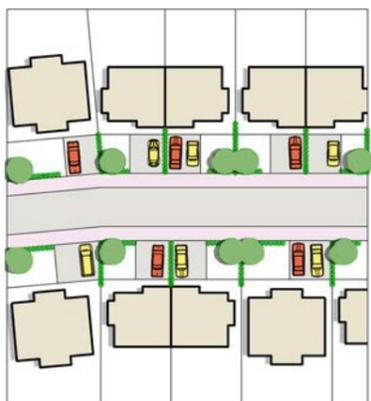
4. Grouped On Street Parking



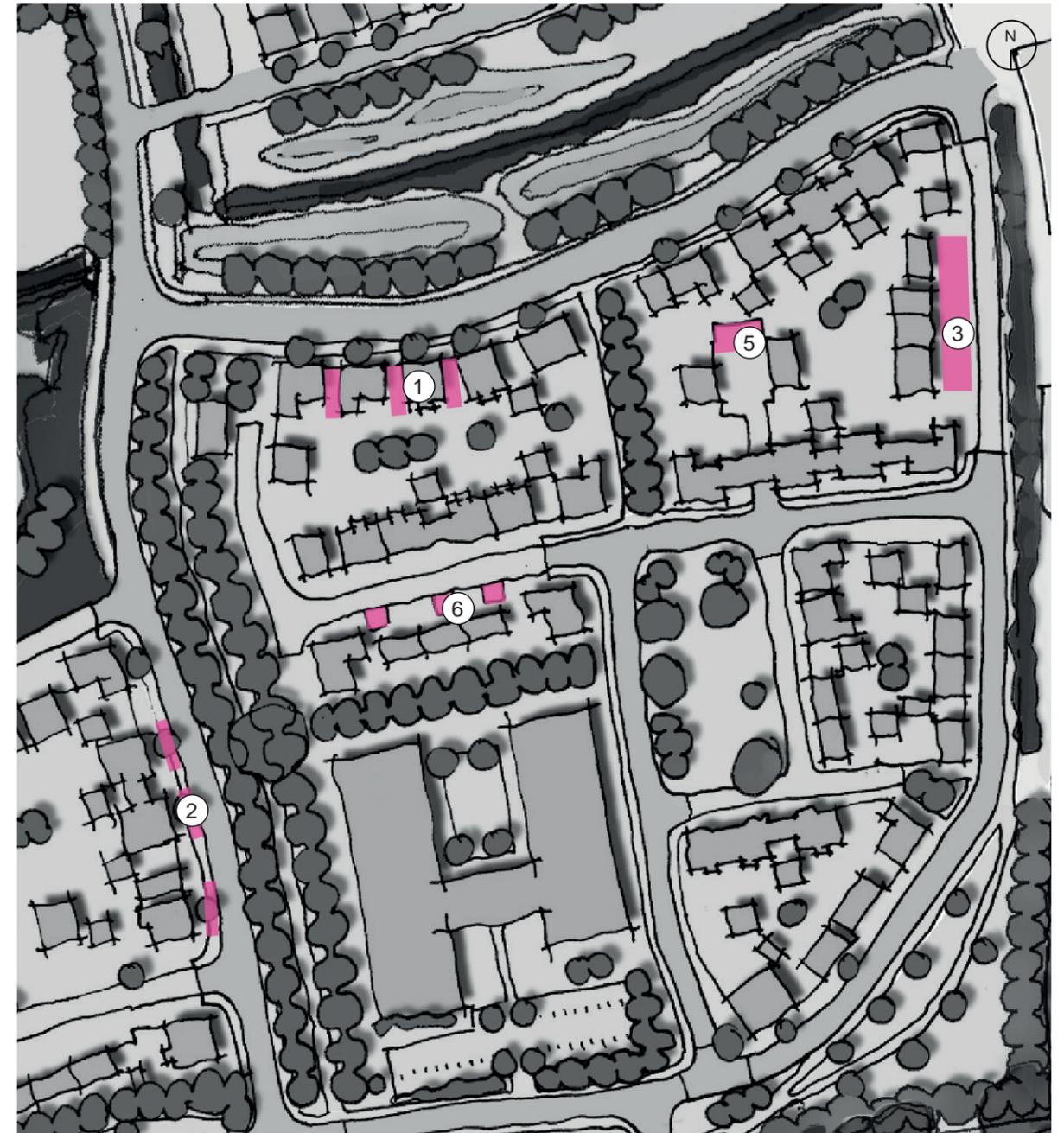
5. Small Rear Parking Courts



6. Front of Plot Parking



Parking Arrangement Diagrams



Typical parking arrangement within a residential area

5.5 Junction and Access

The below plans show the necessary junction improvements needed for the A511 junction and Nottingham Road new access to support a bus route and new vehicular access. The new access at Nottingham Road is illustrated in two ways; both allow buses in each direction.

The first shows how a final junction layout would appear with a complete scheme, allowing limited car circulation south and cars to enter the car park and direction their exit north to the A511.

The second shows how the early phase of development would be served. This is considered an interim solution. It would allow the southern part to function while the link through to the A511 is still being delivered.

These two solutions remain illustrative until a final set of proposals is approved by LCC Highway. See pages 45 and 46 for the vehicle management principles of this area.



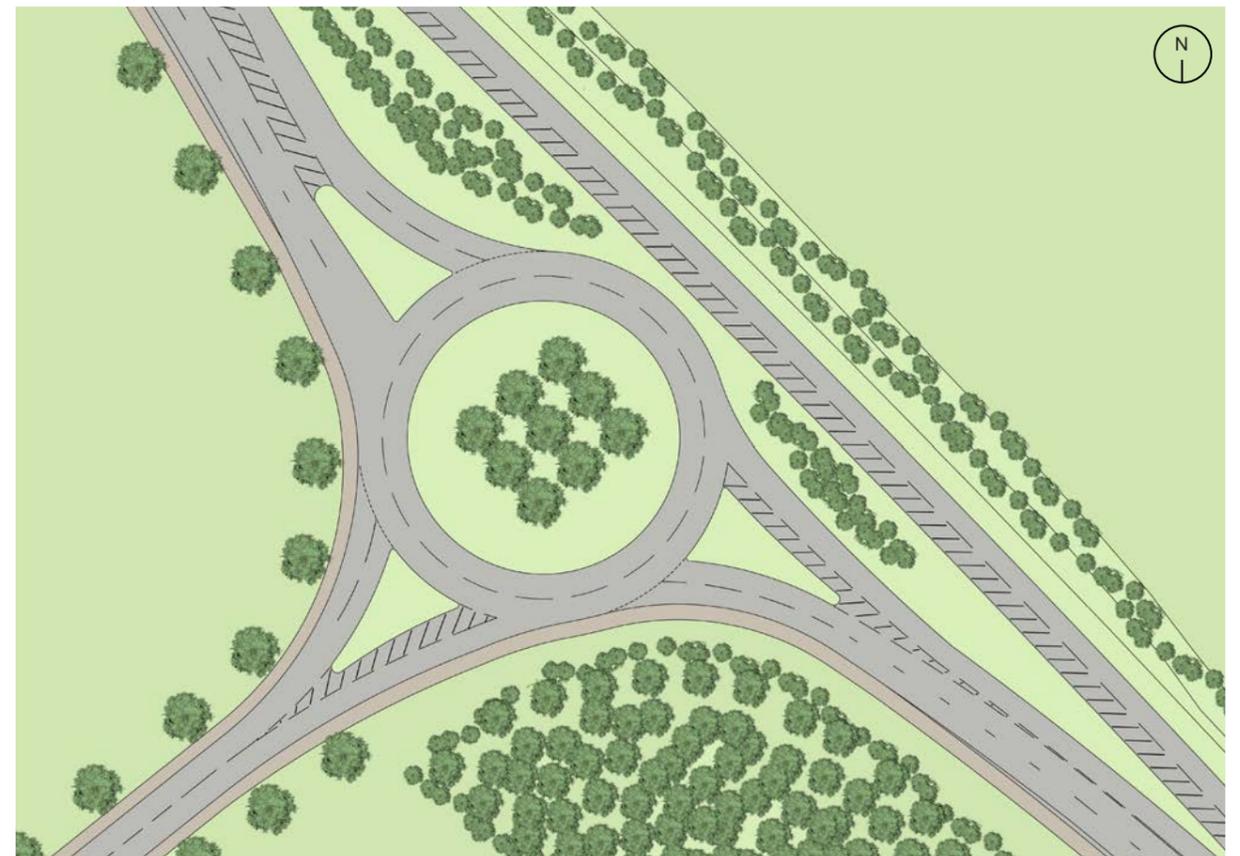
Potential final layout



Potential interim layout



Location Plan of New Junctions



Indicative A511 New Junction Layout Plan

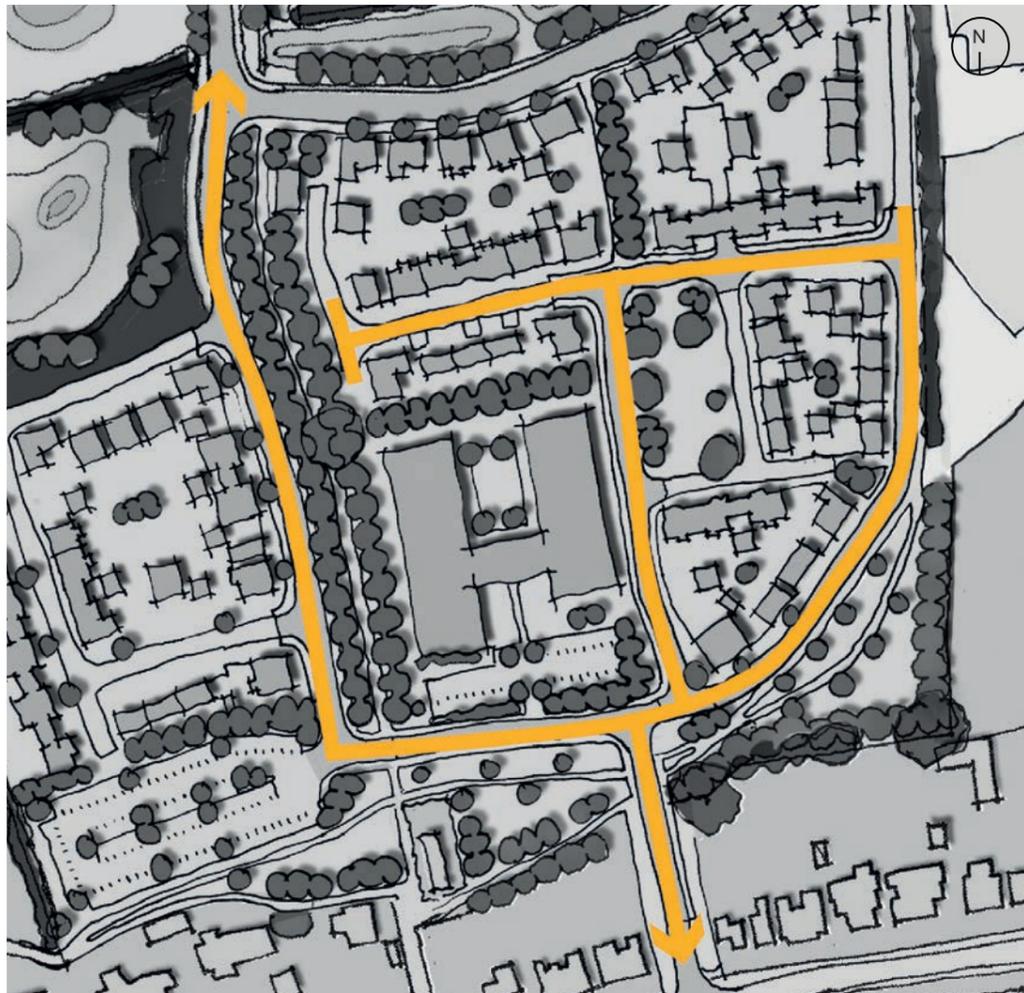
5.6.1 Waste Collection and Recycling

All of the streets in the development must be wide enough to accommodate refuse and recycling collection vehicles. Nonstandard and variable width carriageways are possible, but their detailed design will be expected to demonstrate the acceptability of the proposed widths by showing vehicle tracking on proposal plans. The layout of development in the proposed Masterplan allows for recycling vehicles to remain in forward gear whilst servicing the site. Where cul-de-sacs are proposed, they should include sufficient space for service vehicles to turn. The normal refuse/recycling vehicle to be accommodated has the following specifications:-

- They require a turning circle of 18 metres;
- Their overall length is 9.84 metres;
- Their overall width is 2.5 metres; and
- Their gross vehicle weight is 26 tonnes.

In order to deter the anti-social dumping of waste, properties should be designed to face onto the areas where bin collection is allocated and dead-ends that are accessible by the public should be avoided.

Detailed design will require reference to the council's website, which contains useful information about waste collection and recycling:-
http://www.nwleics.gov.uk/pages/collection_information



Example of a Forward Gear Circulation Route Plan

5.6.2 Waste and the Design of Dwellings

In line with North West Leicestershire policy, dwellings will need to accommodate 4 containers: Blue Bag, Red Box, Brown wheelie bin and Yellow Bag.

The Blue Bag

- Newspaper and magazines
- Junk mail
- Directories (catalogues, phone books, etc)
- Envelopes (please remove plastic windows)
- Wrapping paper
- Shredded paper

Red Box

- Plastic bottles and lids (fizzy pop, shampoo, detergent bottles etc.)
- Household plastics packaging (clean yoghurt pots, plastic tubs and trays)
- Food and drinks cans (clean)
- Aerosols
- Glass bottle and jars (please keep in a separate red box if possible)

Brown Lidded wheelie Bin

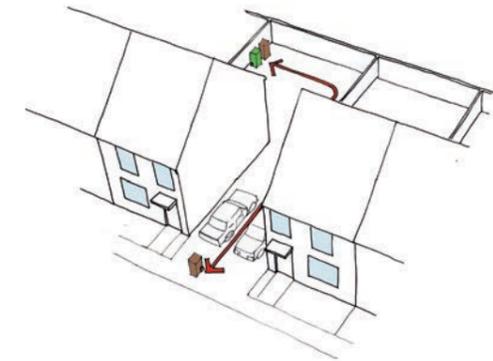
- Shrub prunings
- Garden weeds
- Plants & leaves
- Hedge trimmings
- Dead flowers
- Cut grass
- Small tree branches

Yellow bag

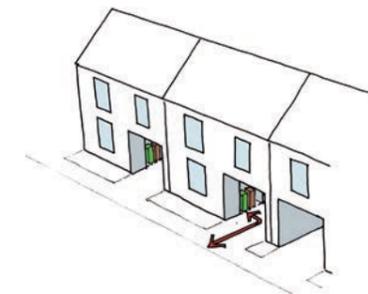
- All types of flattened household cardboard
- Cereal boxes
- Frozen food boxes
- Pizza boxes
- Corrugated cardboard boxes

The design of bin storage systems within Money Hill should ensure that the bins are not visually intrusive and that they do not create a negative impact on the overall setting of the dwellings.

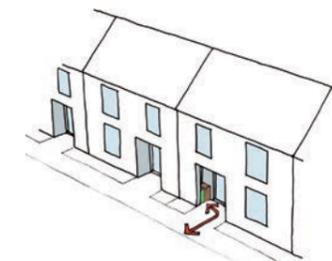
Illustrated below are 3 ways to integrate bins and with houses



Bin storage located to the rear of the property (provided walking distance is no more than 25 meters)



Bin storage integrated into the side flank of the car port



Bin storage within the porch of the house

6. DESIGN SOLUTION

6.1 INTRODUCTION | 6.2 LANDSCAPE | 6.3 ILLUSTRATIVE MASTERPLAN | 6.4 CHARACTER AREAS | 6.5 BUILT FORM CHARACTER
| 6.6 LAND USE | 6.7 AMOUNT | 6.8 SCALE

6.1 Introduction

This section brings together all the analysis of the site, its context and design principles presented in this Design and Access Statement in order to generate a landscape strategy, seven character areas, an illustrative masterplan, built from character and definitive use, amount and scale plans.

6.2.1 Landscape Strategy

The Landscape Strategy comprises a network of accessible open spaces forming a green network across the application site. This structure has developed from understanding the site and relationship with the National Forest (see 2.4.1 and 2.4.2) This green network not only contributes to the creation of a sustainable scheme that can be designed in line with best practice, but it also contributes to creating a setting that is conducive to encouraging healthy living and providing for all activities that are part of family life. The public parks will be punctuated by activity zones designed to cater for a range of leisure activities, ranging from passive uses, walking, sitting, picnicking and to more active areas of play and sport.

Furthermore, the green components of the site will combine with the sustainable drainage system (see Section 4) to provide an environmental infrastructure framework, maximising green linkages and enhancing biodiversity whilst reducing site runoff.

The specific objectives for the landscape strategy are to:-

- Retain the majority of the existing vegetation and boundary trees on and adjacent to the application site and integrate these elements with the masterplan proposals;
- Enhance existing open boundaries of the site with proposed woodland/edge buffer planting to integrate the proposed development into the surrounding countryside to the north;
- Maximise opportunities for habitat creation, biodiversity and wildlife preservation, and the creation of woodland areas as promoted by the National Forest policies;
- Protect the visual amenity of adjoining properties and integrate development into the existing and proposed townscape to the east, south and west;
- Maintain open areas in appropriate locations in order to maximize the contribution of green space to the proposed development and the wider visual amenity of the site; and
- Enhance the setting and amenity value of the Ivanhoe Way running east-west along the south of the site.



Landscape Structure Plan

The detailed landscape proposals reflect the policy guidance, and develop the objectives of the Landscape and Visual Impact Assessment and the Biodiversity Management Strategy and in doing so, emphasise the core functions required of a high quality mixed use and residential scheme.

As well as promoting a natural buffer area to the north of the built form, the scheme creates a series of parkland areas, (with separate identities and functions), the western area being a 'growing park' providing allotment areas within a framework of orchard trees and flowering hedgerows. The central area of public open space will be developed as a formal leisure park with feature ornamental planting of trees and shrubs (as well as native varieties to encourage wildlife), along with "natural" play facilities and seating areas. The southern public open space will be developed in association with enhancements to the Ivanhoe Way, where new woodland blocks will be created to provide screening to the adjacent properties and to create a green backdrop to the Ivanhoe Way.



All the main areas of public open space will be easily accessed by a comprehensive footpath and cycleway network. In addition to the main areas of public open space, a strong north south linear park will be developed around the retained double hedgerow, with new gardens, greened streets and incidental landscaped areas providing a variety of leisure and recreational facilities set within and around the proposed building arrangements and in relation to the landscape buffers, swales and surface run off attenuation areas.



The watercourse corridor, the retained and improved hedgerow corridors and the additional woodland planting forming the 'Moneyhill Park Fringe' open space will create continuous green links within and across the site area landscape, and will be extended to the north eastern edge to meet Ivanhoe Way and connect to recently established National Forest planting along the A511 Bypass and Old Parks Farm.



Creation of wide structural planting belts along with wildflower meadows will form a network of interlinked habitats and will be made accessible through a system of paths and activity zones. Footpath connections to the north will be encouraged, with the exact locations to be determined at detailed application stage. The longer term recommendation is that the Ivanhoe Way running along the eastern edge of the site would be complemented by an additional path, set back from the United Biscuits Warehouse in order to create an attractive amenity setting. Well formed woodland boundary planting along the new path will provide effective screening towards the industrial warehouse and will integrate the new development with the surrounding landscape and setting of Money Hill Park Fringe.



In addition to providing a substantial recreational landscape for the development, the northern and eastern landscape treatment is paramount in setting the built form in the surrounding landscape. Towards the northern edge of the site, rather than simply screening the urban form by continuous tree belts, the built design promotes softened views of rooftops over tree canopies with a corresponding broken building line, with the intention of creating a sympathetic transition from the urban to the rural landscape. Building heights are to be a maximum of 2 storeys with potential for single storey units if required. At the transition from the northern built area to the landscape park, allotments and community orchards will provide opportunities for community activities and social interaction.



Throughout the public realm, street furniture will be contemporary in form, but using traditional materials to complement types used elsewhere in Ashby. A suite of distinctive signage, lighting, litter bins, seating, and recycling facilities will be developed for the site, using robust but refined elements that are easily maintained and replaced when necessary.

Circulation Space

Across the site, the areas of vehicular access will be designed in such a way as to promote pedestrian prominence and safety. A 'Home Zone' or 'Living Street' approach will be taken for all these areas where shared, levelled surfaces, and informal 'door step play' opportunities will be created to provide an attractive, secure, traffic calmed street environment for all users. The creation of convivial places across the development will engender community interaction, and 'eyes on the street' and will compliment local food production and harvesting. A consistent palette of materials will be employed across the public realm areas with feature areas, footpaths and cycleways clearly designated by a hierarchy of paving materials, lighting types and signage. Recreational routes will be of circular character and offer connections to the wider countryside, particularly in respect of linkages to the Ivanhoe Way to the east and Footpath 080 near Smisby Road to the west.

Sustainable Drainage Landscape Treatments

At the detailed design stage, the use of permeable paving types, gap graded sub bases and open channel surface drainage will be detailed to minimise surface discharge from the site and generally slow the passage of water from hard surfaces and if possible remove the requirements for an extensive underground piped system. Unpaved private gardens will be promoted, as will the use of green roof types for outbuildings and secondary structures. The resulting grey – green network will feed into the swales, infiltration beds and attenuation pond system, thereby minimising site discharge and providing a range of habitats for enhancement of biodiversity across the site. On a small scale, 'Rain Gardens' will be introduced in the open space and public realm, providing further opportunities for rainwater collection and biodiversity. For further information regarding drainage see Section 4.



Accessibility

The landscape scheme will be designed to be fully inclusive, and to promote ease of access for all users. Where practical, the public realm and main landscape areas will be either pedestrian only or comprise shared, traffic calmed surfaces. Where level changes occur, ramped access will be provided with maximum gradients of 1:20.

Play and Recreation

The provision of a range of inventive play opportunities and areas where children feel safe to move about will be paramount in the overall design of the outdoor spaces. Within the main body of the scheme a local play area will be designed to cater for younger age ranges from toddler to approximately 12 year olds.

In the wider landscape, informal play spaces and play 'stations' will be created as part of the overall landscape structure. Educational play items and interpretation boards will be provided to encourage playful engagement with local wildlife, ecology and the significant history of the site and surrounding region.

To encourage use of the landscape by adults and to promote healthy living, a circular walk with activity 'trim-trail' stations will be implemented, with off road cycle tracks where appropriate.

Street Trees and Planting

The site wide planting scheme will provide a series of green links throughout and beyond the site boundaries, and will significantly contribute to the aesthetic qualities of the urban form, providing seasonal change and colour, along with a sense of scale and aspect for the building groups and landmark buildings.

The main routes running north-south will be formed by structural avenues and will be greened with boulevard species, such as Field Maple, (*Acer campestre*), Tulip Tree, (*Liriodendron tulipifera*) Lime, (*Tilia cordata* Greenspire), Maidenhair Tree (*Ginkgo biloba*), Magnolia, (*Magnolia Kobus*), Hornbeam (*Carpinus betulus*), providing strong green 'statement' street vistas. Throughout the main road system, the creation of green interludes in the form of parkland or woodland crossing points (Moneyhill Park Fringe and Brook Lanes), and more formal hard and soft spaces such as School Square and Ivanhoe Square will be supplemented by smaller 'openings' of the road alignment in the form of minor squares, enlarged verges, informal seating areas, (in association with the main retained hedgerow) and broadened junctions.

The secondary street and lane systems will be greened with medium sized species, often forming tighter avenues and single sided planting arrangements. The sustainability aspects of buildings and solar gain are to be considered carefully to allow appropriate levels of street greening, but also to promote good practice in terms of building environments and interior room conditions. Typical species might include; Whitebeam, (*Sorbus aria* 'Majestica' and *Sorbus aria* 'Magnifica'), Hawthorn (*Crataegus laevigata* 'Paul's Scarlet'), Silver birch, (*Betula pendula*), and Holly, (*Ilex aquifolium*).

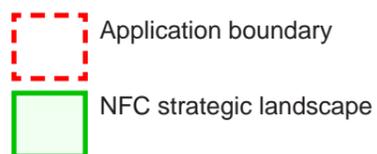
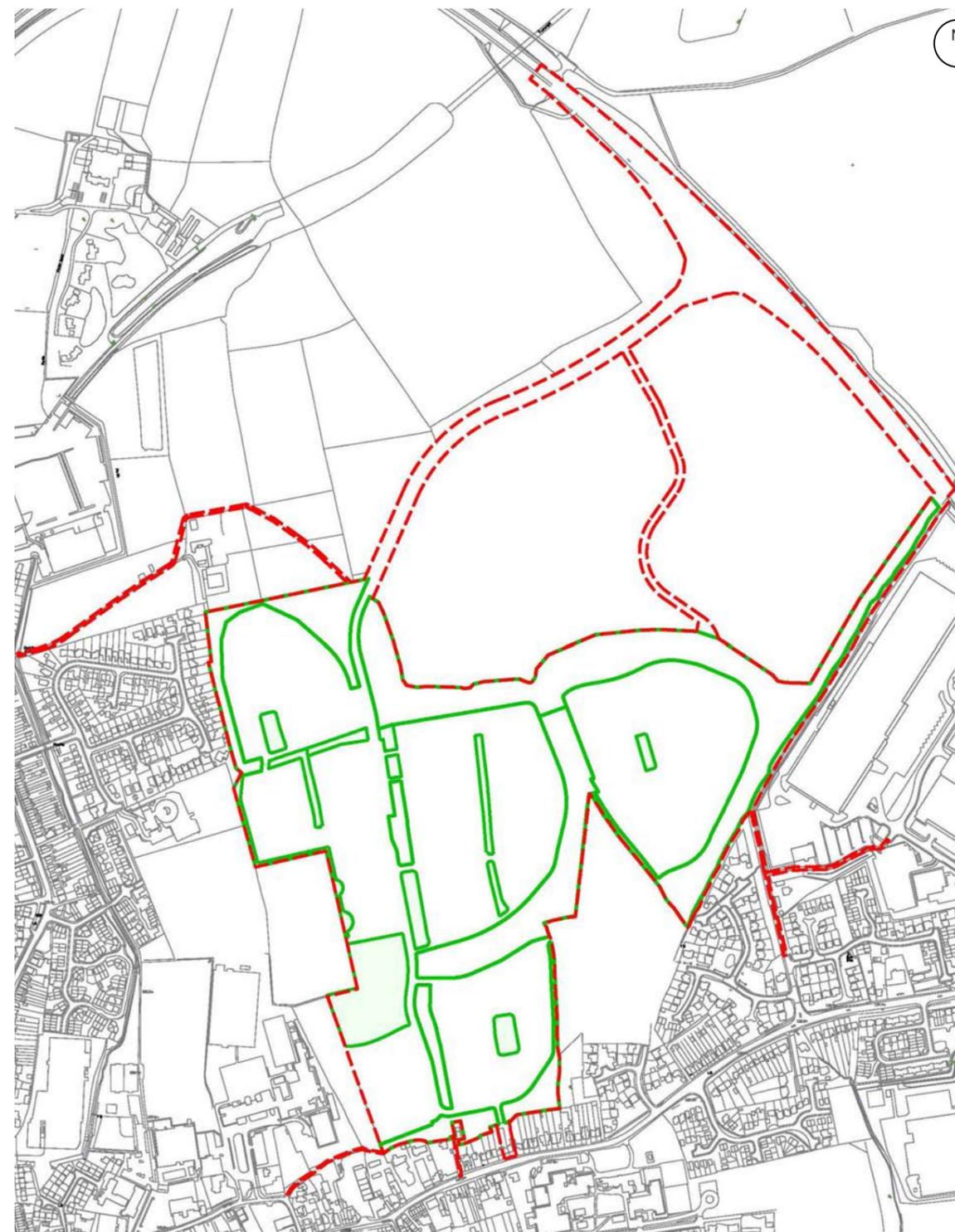
Softworks designs using native grasses and herbaceous in combination with more traditional planting elements will be encouraged throughout the urbanised area. A percentage of native evergreens will also be included in these mixes to promote winter time screening, along with pioneer, faster growing species to create early impact. The long term maintenance regime will carefully develop the boundary screening to promote good structure and longevity and therefore ultimately contain the built form with a collar of green.

6.2.2 National Forest Requirements

The NWLDC policy states that 33% of a development will be required for the provision of strategic landscape. The NFC refers this type of landscape as ‘Green Infrastructure’ and includes creating new and managing existing assets such as woodland, landscaped areas with trees, wildlife habitats, recreation areas and routes, wetland and landscape features.

The planning application area is 42.03Ha which includes a width for the existing A511. Excluding access arrangements the Gross Development Area is 35.36Ha. This development area would require a 33% NFC strategic landscape as 11.67Ha.

	Ha		
App area	42.03		Anticipated over supply of NFC
Access arrangements not included in sum	6.67		
App area excluding access	35.36		
Strategic landscape based on Land Use Parameters Plan	12.31	33%	

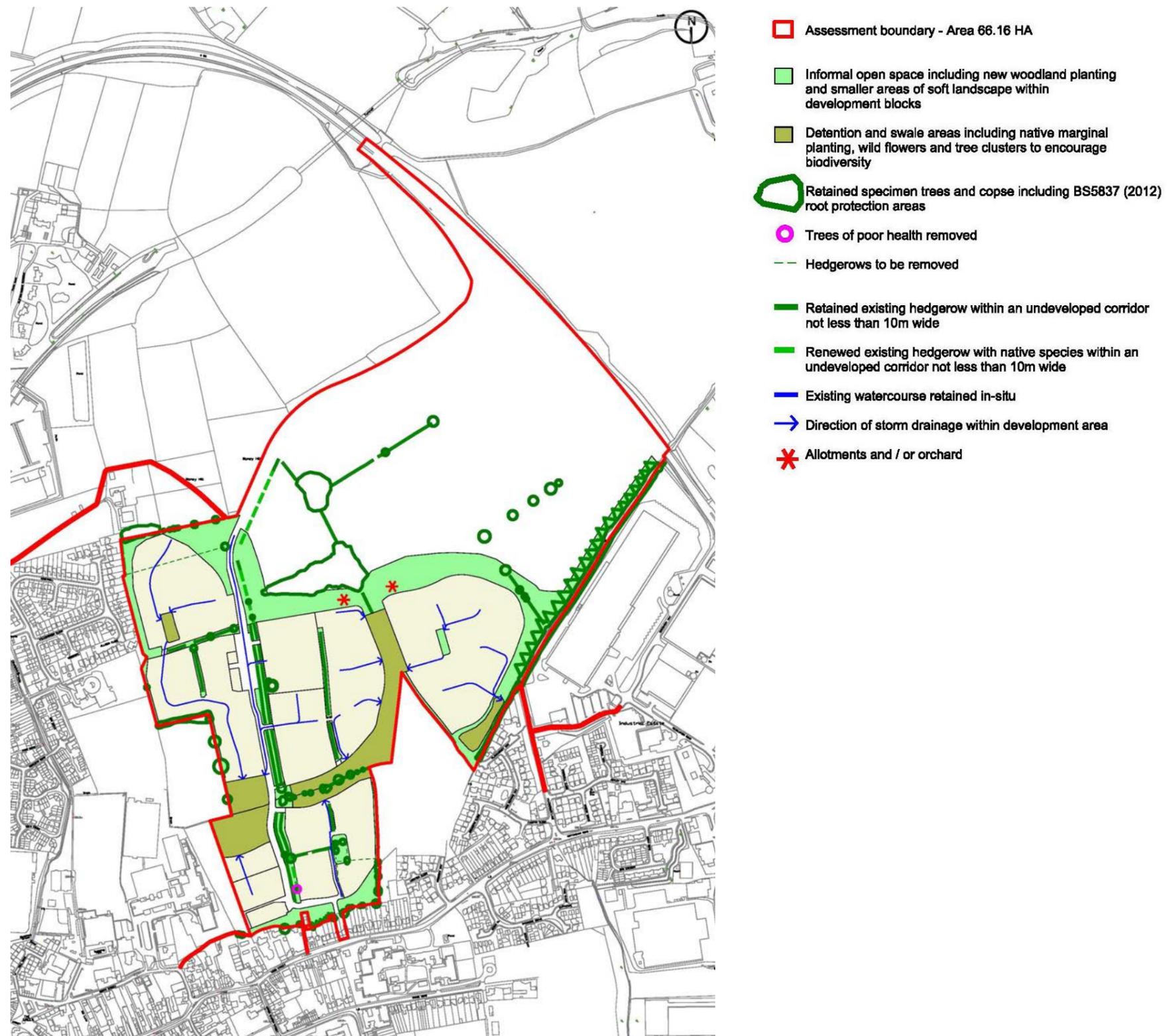


Open Space Plan

6.2.3 Landscape Structure

The Money Hill development proposals seek to introduce a large amount of newly created public open space. Many of the open spaces proposed are multifunctional, incorporating recreational space, play areas, sustainable drainage and improved on site biodiversity.

The creation of open spaces at Money Hill will also result in a new, high quality landscape asset for Ashby that is close to the town centre and easily accessible for both new and existing residents to use and enjoy. The open spaces are positioned to align with the pedestrian and cycle network (see 5.2.3).



Landscape Structure Plan



Illustrative Masterplan (original scale 1:2000@A1)

6.3 Illustrative Masterplan

The adjacent plan provides an indicative overview of the proposed masterplan. This plan should not be considered as an accurate reflection of a final solution, but will allow the local authority to consult on the detail design at a later stage.

Key points of the plan include:

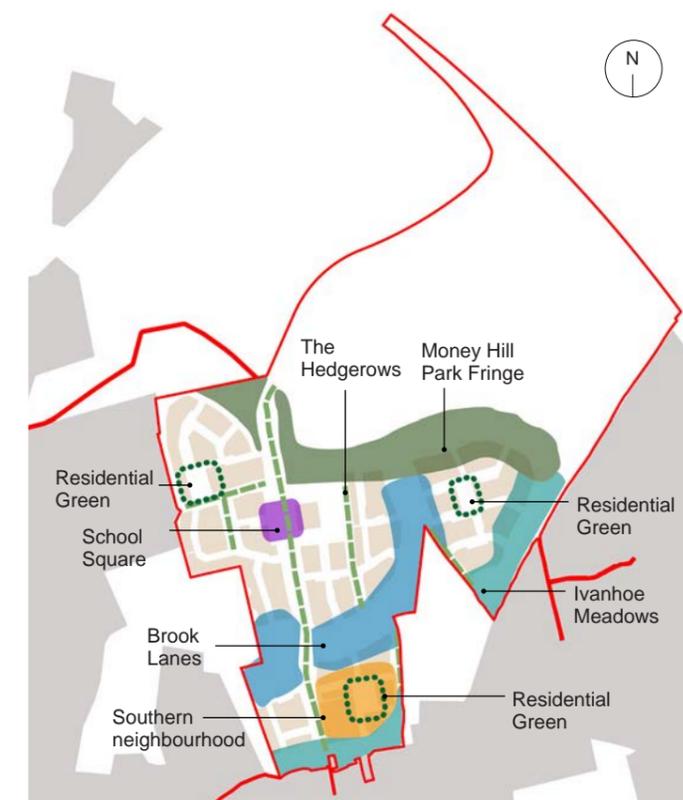
- Enhancing of the setting of Ivanhoe Way and the set back from existing properties on Nottingham Road.
- New Primary School and local centre area with potential for nursery and local shop.
- A central multi- functional space with sustainable drainage, improved wildlife habitats and site biodiversity, and new amenity space.
- A sequence of hedgerow corridors creating a woodland setting for the whole site.
- A series of 'Greens' within residential areas providing amenity and areas of play for local residents.
- Possible allotment and orchard areas set between the double hedgerow near local centre area and located at Money Hill Park Fringe.

This proposal will provide much needed housing within the area, as well as new high quality public open spaces and landscape connections to the wider National Forest landscape.

6.4 Character Areas

There are a series of character areas that have been created to provide a sense of place to the Masterplan. The character of these areas has been generated through the retention of existing landscape features, the movement network and proximity to the town centre. The following section (6.4.1 - 6.4.7) looks at these character areas in more detail and explains the potential built form and landscape design solutions.

- 6.4.1 Ivanhoe Meadows
- 6.4.2 Southern Neighbourhood
- 6.4.3 School Square
- 6.4.4 Brook Lanes
- 6.4.5 The Hedgerows
- 6.4.6 Residential Greens
- 6.4.7 Money Hill Park Fringe



Location Plan of Character Areas

6.4.1 Ivanhoe Meadows

Ivanhoe Meadows is a key landscape area in the proposals for Money Hill, intended to create a welcoming and distinctive character at the southern entrance to the site. Its main landscape component is a wooded meadow that runs along the southern and south-eastern edges of the site, creating a new landscape setting for Ivanhoe Way and an attractive buffer between the existing and new properties. All existing pedestrian access points will be retained and improved creating a comprehensive movement network (see 3.1.5 and 5.2.3). Its open meadow appearance will reflect the rural location of the town and the nature conservation aspirations of the National Forest Company, whilst the buildings fronting onto it will express the built character of the new neighbourhood.

The meadows will provide multifunctional open space incorporating Ivanhoe Way National Trail (see 2.4.4), a new woodland meadow setting, sustainable drainage and the southern entrance gateway to Money Hill.

The character of the built form fronting onto the Ivanhoe Meadows area will be defined by the following features:-

- The buildings that front directly onto the open space of Ivanhoe Meadow will create a high quality entrance to the Money Hill development. The houses will be arranged in a gentle crescent to create gateway feature and sense of arrival;
- Soft boundary treatments, including front gardens planted with trees and low hedgerows, will be used to define the public realm (see 6.5.3 for further information);
- Building heights will be limited to between 2 and 2.5 storeys to reflect the amount and scale principles set out in 3.2.2;
- The houses will predominantly be arranged as detached and semi detached dwellings in order to create an informal woodland edge setting;

- Individual houses will feature gable ends and bay windows to maximise key views across the open spaces.
- Architectural features such as chimneys and materials such as red brick, part render and slate roofing will be used to reinforce the local sense of identity and to reflect the existing architectural character of Ashby (see 2.3.2); and
- Timber detailing may be considered for key buildings to reinforce the National Forest character (see 6.5).
- The street will be suitable for bus access and have a footway and off highway cycleway (see 5.3.1 Street Hierarchy Bus Route). Appropriate landscaping and frontage treatments will help to distinguish this important movement route.
- Ivanhoe Meadows will be an accessible open space for existing and new residents with improved pedestrian links to Nottingham Road and improved surfacing to Ivanhoe Way.



Cross Section Location Plan

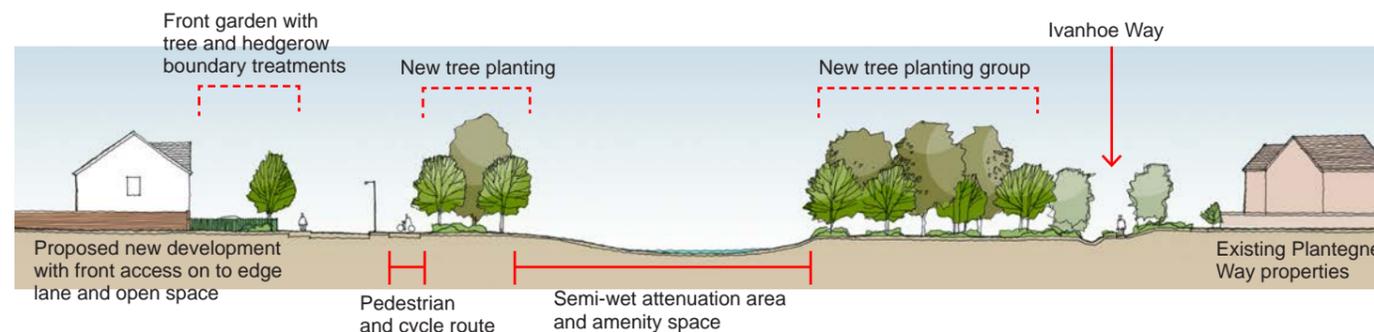


Informal arrangement of detached houses fronting on to open space

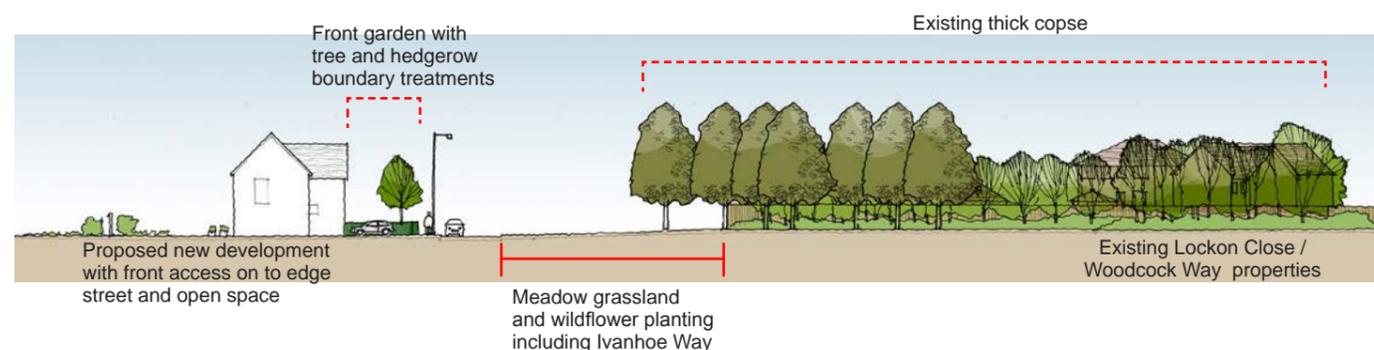


Dwellings with gable features creating an interesting edge to an open space with newly planted tree groups

A A cross section of Ivanhoe Meadows



B B cross section of Ivanhoe Meadows



The landscape character Ivanhoe Meadows will be defined by the following features:-

- Informal clusters of trees with selected large specimen trees at focal points;
- Intermittent grassed areas with swathes of wildflowers;
- Inclusion of trees in front garden to ensure a tree lined setting for Ivanhoe Way;
- Soft boundary treatments including front gardens with small specimen trees and low hedgerows (see 6.5.3 for further details); and
- Bulb planting to create seasonal accents.



Example of reed and grass planting around attenuation area



Meadow style public open space with informal tree planting and grassed areas



Sketch illustration of Ivanhoe Meadows



Sketch illustration of Ivanhoe Meadows

6.4.2 Southern neighbourhood

The new southern neighbourhood will provide a sense of arrival to the masterplan from Nottingham Road.

The southern neighbourhood will be distinguished by a green setting for Ivanhoe Way. An alternative footpath will be provided so that the setting of the existing cottage is enhanced. Both the existing and the alternative paths will be provided with bound gravel. In addition, the shrub and tree planting to the north of the rear garden boundaries of Nottingham Road houses will be increased. These attributes are illustrated in the sketch below.

The layout of the southern neighbourhood will acknowledge and contribute towards providing a sense of place when approaching from Nottingham Road. This will be done by providing an area of informal landscape to form a set-back of development from existing houses along Nottingham Road. Sufficient space will be included for a potential extra care facility and for this to be visible from the southern approach. The layout will also ensure good legibility for drivers whether approaching the car park, housing or potential extra care. Furthermore, a village green in the place of the old fruit trees, will be included. It will create a centre for the southern community.

See pages 45 and 46 for the vehicle management principles of this arrangement.

Existing cottage



Sketch Illustration of Ivanhoe Way new landscaped area



Southern neighbourhood illustrative masterplan

The illustration on this page is a representation of the new Nottingham Road access arrangements, looking west from the junction of Wood Street and Nottingham Road.

The gateway buildings for the new street will be formed by the existing no's.1 and 7 Nottingham Road. Soft landscape verges will form part of the landscape setting of the entrance.

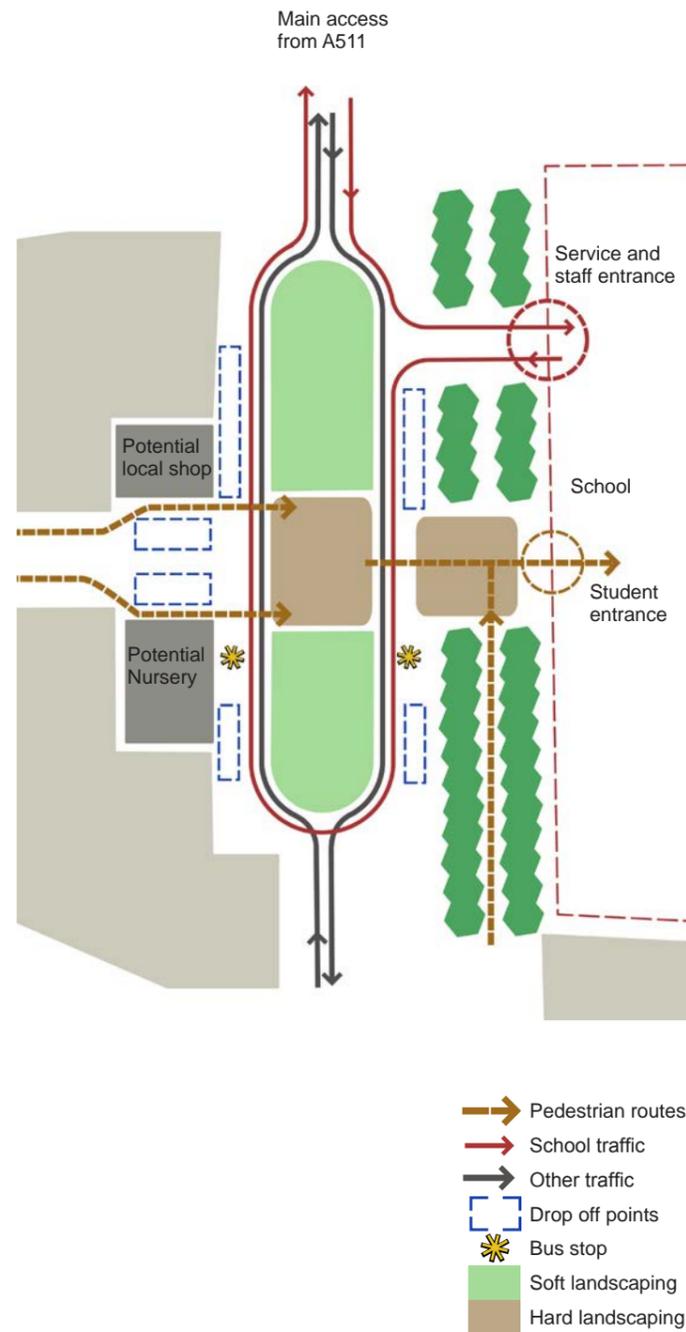
The Nottingham Road bus stop will be moved slightly in response to the setting of the new junction .



Sketch illustration of new access at Nottingham Road

6.4.3 School Square

A community hub for the new Money Hill neighbourhood is proposed for the area in front of the new school. This is known as School Square. It has to be capable of accommodating many different functions associated with the use of the school, both functional and social. For example, it needs to include safe school drop off points and a bus stop, and the potential for a local shop and a nursery space as well as residential dwellings. The School Square concept plan shows how pedestrian, cycle and car circulation could be managed within the space, giving priority to pedestrians routes.



The School Square concept plan shows how pedestrian, cycle and car circulation are managed through the space giving priority to pedestrians movement.

The character of the built form around the School Square will be defined by the following features:-

- The street that runs along the frontage of the school is the main access into the new Money Hill neighbourhood. The alignment of this street will therefore require a number of traffic calming features. The specific section in front of the school must contribute towards speed attenuation, whilst also accommodating a number of other activities;
- The school entrance will generate a lot of pedestrian activity, particularly at school opening times. Through detailed design, the space immediately outside the school entrance will allow for pedestrian circulation, particularly along the important east-west desire lines;
- There will be sufficient space to allow for parents to use their cars to drop off and pick-up their children, either in the carriageway or in dedicated car parking pockets;
- A bus stop will be located on each side of the square so that the community hub uses are easily accessed by public transport. These bus stops will also allow the school to charter coaches for trips;
- The vehicle circulation space will allow for the school to be serviced and for staff to gain access. It should also be convenient for vehicles travelling north, wishing to access the A511;
- All buildings will provide active frontages and direct front door access onto the street and square (see 3.2.3 for layout principles); and
- In support of the National Forest Company's identity and the educational potential of the development, the school could be used to showcase new sustainable building practices such as the use of locally sourced timber, grey water harvesting systems and green roofs.



Example of a contemporary new school building with timber cladding, set behind an informal shared space



Contemporary timber street furniture and lighting

The landscape character of the School Square will be defined by the following features:-

- There are a number of preserved hedgerows in the area. These will be retained and enhanced but will be breached to allow school access both pedestrian and vehicular;
- Multi-functional use of space will be reflected in materials and structural planting;
- There are a number of trees in the area that would merit retention. Layouts of development and infrastructure will respect the root protection zones of retained trees;
- Informal play in pedestrianised parts; and
- Promote the character of a neighbourhood square, combining hard landscape with formal tree planting and a central lawn;
- Use of ornamental shrub and herbaceous planting to introduce colour and seasonal variety.



Sketch illustration of the pedestrian crossing point at School Square



School Square Layout Plan

6.4.4 Brook Lanes

The proposed Brook Lanes open space forms a central landscape corridor following the course of the existing watercourse at Money Hill.

Primarily, this space is proposed as an area of sustainable drainage, including swales and attenuation areas. Not only will these measures provide essential attenuation of surface water run off; they will also contribute significantly to site biodiversity and to the visual amenity of the development. It is proposed that the western end of the corridor widens to include an attenuation area of shallow basins and semi-wet grasslands.

The character of the built form around the Brook Lanes will be defined by the following features:-

- Buildings will be grouped in informal arrangements with varied building set backs to create a soft edge to the open space;
- Building heights will be limited to between 2 and 2.5 storeys to reflect the amount and scale principles set out in 3.2.2;
- The houses will be arranged as a mix of short terraces and detached and semi detached dwellings in order to create a semi rural edge;
- Residential lanes and shared driveways will run alongside the open space to ensure that there is a continuous active frontage overlooking it and to minimise the need for rear parking courts;
- Any edge driveways must retain continuous pedestrian and cycle access links so as to ensure uninterrupted connectivity for walkers and cyclists;

- Wherever possible, buildings will have vegetated front gardens planted with trees, hedges, grass and soft planting; and
- Architectural features such as gable ends, bay windows and balconies will be used to maximise key views across the watercourse corridor and provide natural surveillance to the open space.

The landscape character of the Brook Lanes will be defined by the following features:-

- Connecting footpaths and footbridges over the watercourse to promote north- east permeability;
- Extensive wild meadow areas with mown paths;
- A circular path network around rain water attenuation and detention areas;
- Woodland edge planting with native ferns, grasses and tree species; and
- Seating and play for informal recreation in appropriate well overlooked areas



Sketch illustration of the multi-functional space along the Brook Lanes incorporating sustainable drainage, leisure and amenity activities



Example of an edge lane with soft boundary treatments and shared surface carriageway



Example of dwellings overlooking an attenuation area with tree and hedgerow planting



Example of a timber footbridge over a watercourse

6.4.5 The Hedgerows

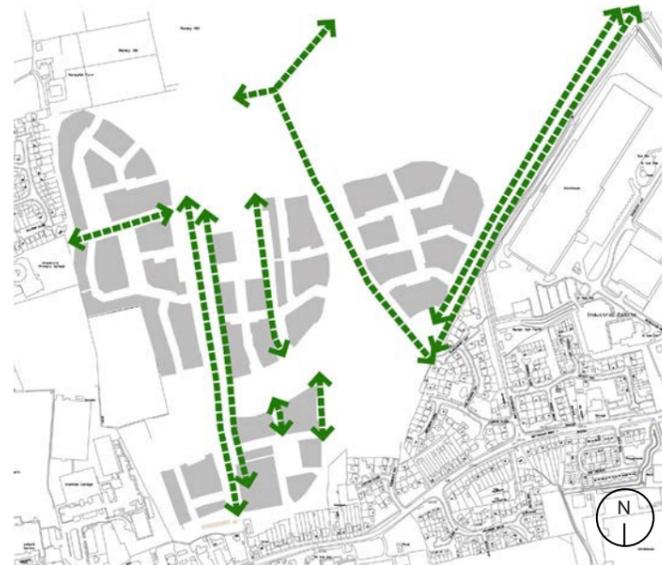
The retention of existing hedgerows forms an important part of the proposed Masterplan and impacts significantly on street and housing layouts across the whole site. The relationship between built form, streets and hedgerows generates a distinctive character that contributes to the woodland identity of Money Hill (as set out in the vision see 1.2).

The character of the built form along the Hedgerows will be defined by the following features:-

- Groups of buildings will front onto and overlook the hedgerow corridors, creating a series of green Edge Lanes (see 5.3.2 for cross section of Edge Lane) running through the neighbourhood;
- Typical Edge Lanes and private drives must retain continuous pedestrian and cycle links so as to ensure uninterrupted routes for walkers and cyclists; and
- Buildings will have soft landscaped boundary treatments.

When selecting access types to use for individual dwellings, subsequent designers will be expected to refer to Leicester County Council's 6 Cs Design Guide to gain a full understand the consequential impacts on parking. The NWLDC Residential Design Guide SPG should also be referred to. This is particularly important in relation to the Hedgerows because if there is only footpath access to the front of a building, parking will be required to the rear, and rear parking arrangements require adherence to additional design criteria.

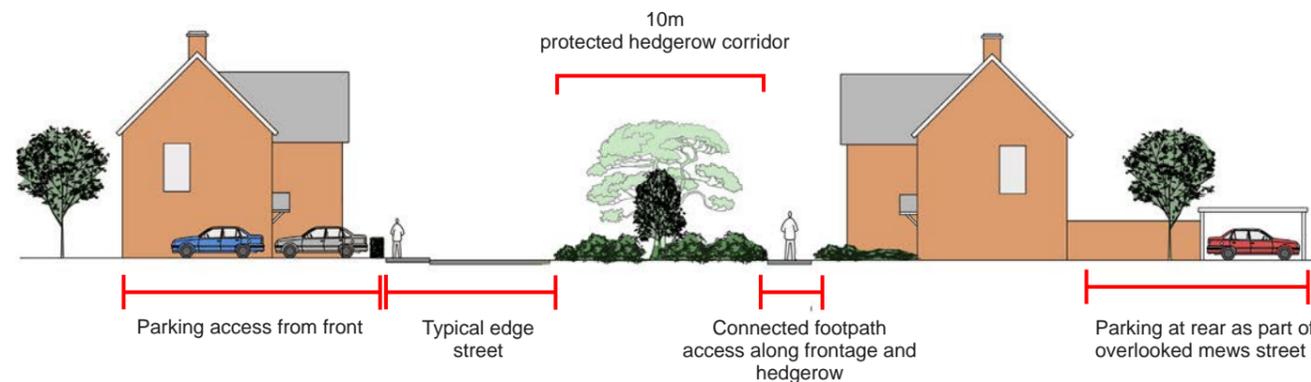
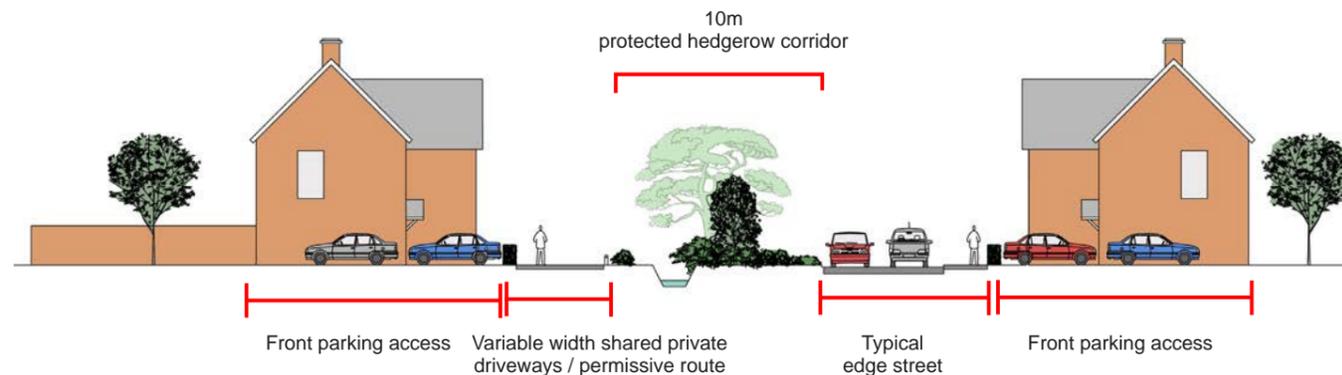
The two cross sections on this page demonstrate the different impacts of front or rear access in relation to the hedgerows and illustrate how pedestrian connectivity, car access and servicing all need to be considered. In all instances, a 10m landscape corridor is retained.



Hedgerow Corridor Plan

The landscape character of the Hedgerows will be defined by the following features:-

- The hedgerow will be 'no build' corridors not less than 10m wide and will form part of the public realm.



The above diagrams show how buildings, streets and pedestrian only routes will be set back from preserved hedgerows



Edge Lane along mature hedgerow



Edge private drive with permissive footpath connections



Footpath edge treatment along hedgerow corridor

6.4.6 Residential Greens

A series of small greens are proposed within the residential areas of Money Hill. These informal spaces are located at three key locations in the southern, eastern and western neighbourhoods. They will be multifunctional spaces providing amenity space, opportunities for informal play and sustainable drainage.

The character of the built form around the Residential Greens will be defined by the following features:-

- Buildings will be used to provide a strong sense of enclosure to each of the open spaces, creating a contrasting character to that of the larger areas of open space such as the Brook Lanes;
- The houses will be arranged to frame the spaces and will have their front doors and windows oriented to provide natural surveillance;
- Depending on the surrounding context, the greens will be surrounded by a mix of short terraces and detached and semi detached dwellings. This variety will create visual interest around the open spaces;
- The boundary treatments used will reflect the woodland identity of the development and will include low hedgerows or shrub planting (see 6.5.3 for frontage treatments);
- Building heights will vary depending upon location but most will be limited to between 2 and 2.5 storeys;
- Individual houses will feature gable ends and bay windows to maximise key views across the greens; and
- Architectural features such as chimneys, and materials such as red brick, part render and slate roofing will be used to reinforce a sense of local identity and to reflect the existing architectural character of Ashby.

The landscape character of the Residential Greens will be defined by the following features:-

- Native tree and shrub planting will accentuate existing trees and hedgerows;
- The addition of herbaceous and flowering boundary planting will create a more intimate character;
- Informal seating will be arranged around elements of doorstep play;
- Central lawns form the setting to accommodate amenity uses;
- Well defined pathways will provide access to the open spaces; and
- A distinctive palette of materials is given to the hardworks elements such as seating, lighting, footpaths and litter bins.



Residential Greens Location Plan



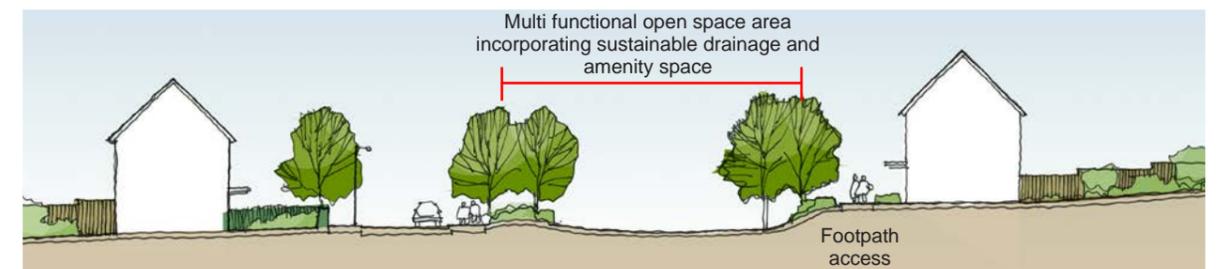
Sketch illustration of the southern Residential Green



Example of suitable play equipment



Example of timber fencing and new hedgerow planting with stile feature



Cross section of the western Residential Green

6.4.7 Money Hill Park Fringe

The Money Hill Park Fringe is proposed to form a new community park close to Ashby town centre. It will retain an existing copse area and introduce new areas of woodland planting. It is expected to become a key leisure area, featuring walking and cycling routes through the landscape.

The character of the built form around the Money Hill Park Fringe will be defined by the following features:-

- Buildings will be arranged in informal groupings with varying set backs to create a soft edge to the park;
- Building heights will be limited to between 2 and 2.5 storeys;
- A rural edge to the park will be created by an arrangement of detached and semi detached dwellings;
- Residential lanes and shared driveways will be integrated alongside the park in order to ensure that there is a continuous active frontage overlooking it and to minimise the need for rear parking courts;
- The design of any driveways along the edge of the park must retain continuous pedestrian and cycle links so as to ensure uninterrupted access for walkers and cyclists; and
- Wherever possible, buildings will have vegetated front gardens, planted with trees, hedges and grass.

The landscape character of Money Hill Park Fringe will be defined by the following features:-

- Structural woodland clusters with edges of native shrub and hedgerow planting will create connections with the wider landscape and is intended to form part of the National Forest;
- The central retained woodland will be included in the park and treated as an important ecological entity with a small number of paths directed through it;
- A belt of native planting will be established, along with areas of wildflower grasses, bulbs and herbaceous planting to create interlinked habitats;
- A fragmented copse structure will be achieved by creating gaps in the tree planting to allow open views into and out of the site;
- Circular walks and a trim trail will provide a network of continuous recreational routes;
- A new planting palette of native trees and shrubs is introduced to complement existing tree and hedgerow species and define important elements of the landscape;
- Informal seating and picnic areas will give opportunities for rest and lookout;
- Areas of play will be integrated in the wider landscape structure;
- Footpaths and cycle routes are provided for wider access to the countryside and connecting to Ivanhoe Way; and
- Allotments and community orchards with facilities to encourage community activities will be included in the recreational setting.



Sketch Illustration of Money Hill Park Fringe



Examples of informal pathways in a woodland setting

6.5.1 Principles of Built Form Character

The appearance of the Money Hill Masterplan will be generated from a series of built form principles that have taken influence from Ashby and the National Forest Company aspirations. These principles will help create a distinctive character and sense of place with particular attention to materials and building frontages.

Ashby's architectural character principles as set out in 2.3.2 include:-

- The use of gables;
- Chimney stacks;
- Varying building set backs;
- Symmetry and proportion;
- Bay windows;
- Archways through buildings; and
- Red brick, white half render and slate roofing materials.

Character principles to reflect the National Forest Company (see The National Forest Design Charter) include:-

- Use of timber in building design;
- Incorporate green roofs where appropriate (see 4.3.5);
- Incorporate rainwater harvesting systems (see 4.3.6); and
- Include soft landscape treatments to building frontages including, hedgerows, trees in front gardens, grass and shrub planting and estate and timber fencing.



Carriage entrance through building to allow for continuous frontage



Mix of half timber / half brick facade detailing to provide variety to streetscape



Variations in roof gable details

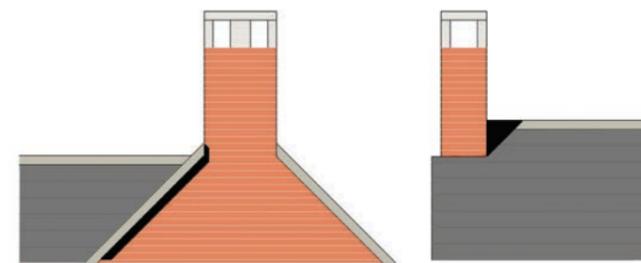


Timber building elements to enhance streetscape

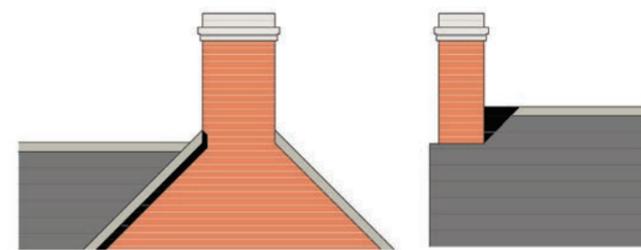
6.5.2 Materials

In order to be considered good design, subsequent detailed proposals will need to rely on materials that are acknowledged as being suitable for the area, including brick from the clay sub-soil, timber from the time of the forest and slate from the time of the railways. The materials in this pallet can afford both traditional and contemporary interpretations. However, the selection of materials should avoid a confused application of architectural styles or inappropriate historical imitation. In particular, the miss-use of recreated period details could not only give the impression of an applied veneer, it could also detract from the genuine historic character of Ashby.

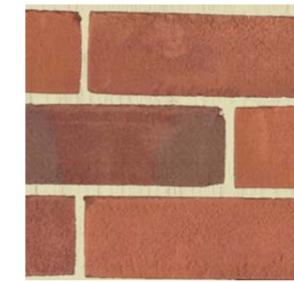
Careful detailing of the ways in which building materials will be applied often ensures that they retain their qualities in the long term. For example, taking account of weathering and how materials will function and appear in 10 to 20 years time will make a better building at the outset.



Illustrative contemporary chimney stack detail



Illustrative traditional chimney stack detail



Multi red brick



Slate tile roof



White / cream render



Green roof



Timber cladding



Multi red tile roof



Timber cladding end gable detail

6.5.3 Frontages

Boundary features that define streets and proposals for the appearance of front gardens are of equal importance to architectural design. When laying out these areas, particular care ought to be taken over openings, gates, corner returns wrap around corners and surfaces.

At Money Hill, where setbacks are wide enough to park a car, there should be at least an equal area of green landscaped space. To enhance the garden character of the proposed streets, tree planting will be expected within front gardens. This approach will ensure that trees can be used to enhance the characters of the streets without the need for them to be located within the adopted highway.

In the Primary Streets, it is proposed that hedges will form the front boundaries of properties, subject to pedestrian visibility.

Away from these busier streets, the front gardens will be narrower and will have soft landscaped verges within the curtilage of the properties.

Rural boundaries will be timber or estate railings.



Tree planting in front garden with hedgerow boundary treatment



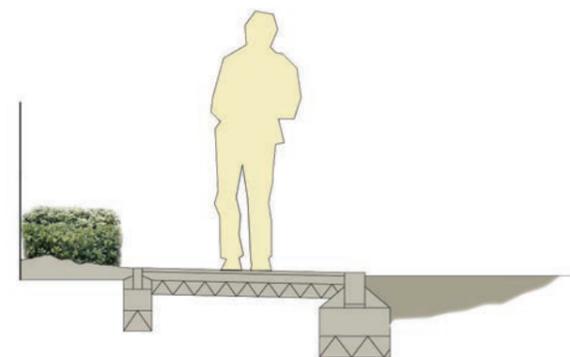
Soft landscaped boundary treatment



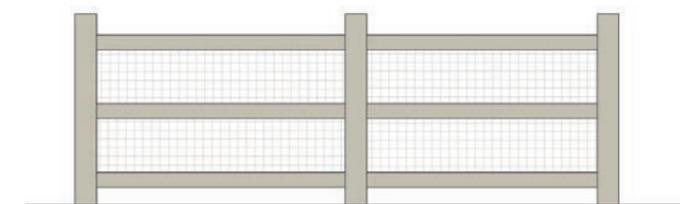
Simple planting within narrow set back with no boundary treatment



Rustic timber post and rail boundary treatment with hedge planting



No boundary treatment with shrub or hedge planting



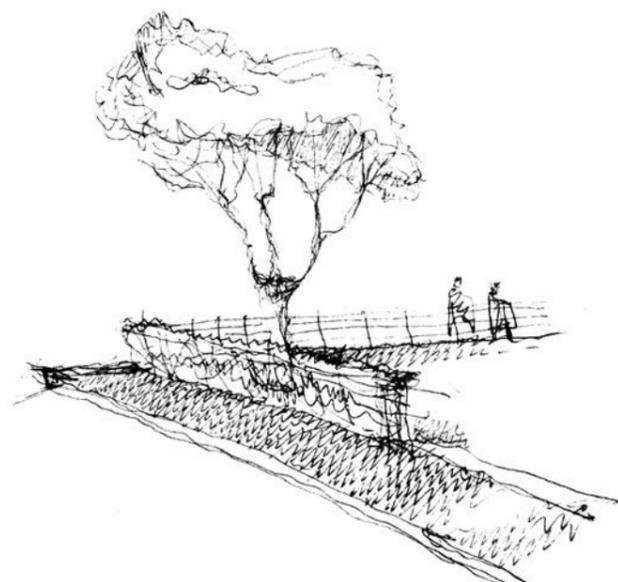
Typical timber post and rail fencing



Hedgerow boundary treatment



Typical estate railing boundary treatment



Sketch of front garden tree and hedge enhancing streetscape, without the need to be located within the adoptable highway

6.6 Land Use

The adjacent plan shows the proposed land uses for the site. It shows that Money Hill will deliver a mix of uses near to the existing town centre, but the majority of the site will be comprised of residential development with a mix of two to five bedroom dwellings.

The land use positions in the masterplan have been informed by existing land uses (see 3.2.1 Design Principles: Use) and the existing and proposed movement network (see Section 5).

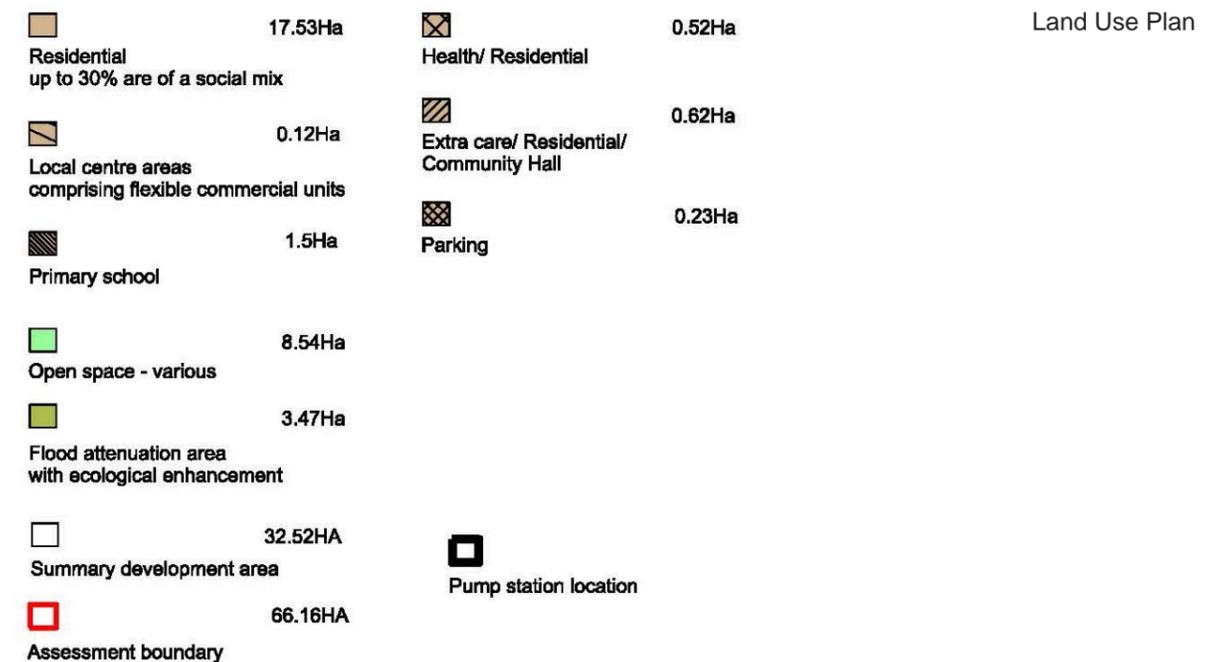
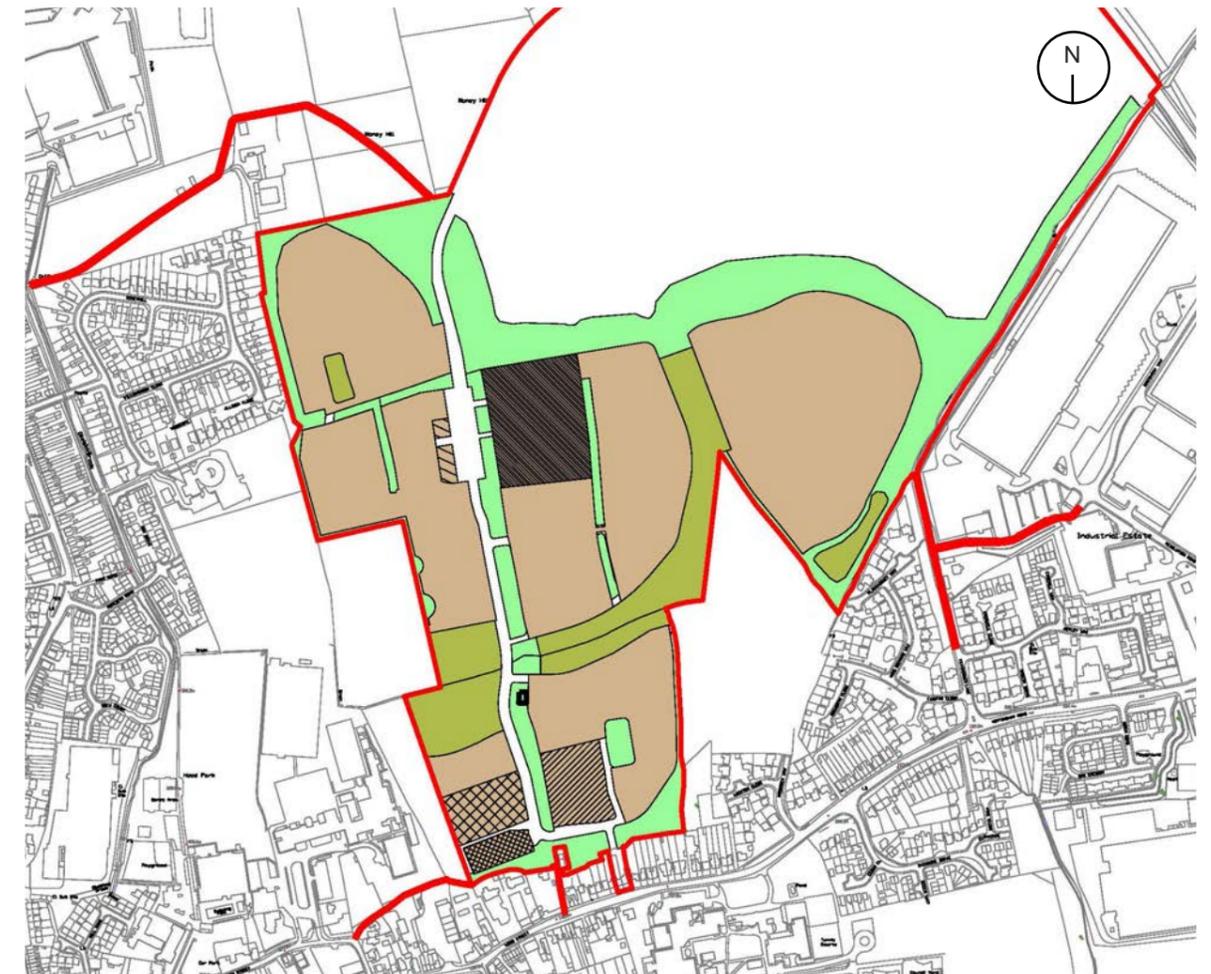
A new primary school will be provided on the site and will be constructed by the Money Hill Consortium. This new school will have the capacity for 210 students. It will be centrally positioned to best accommodate the proposed development and it will be served by direct bus access. Other uses located nearby will potentially include a nursery school and some local centre uses.

To encourage walking and cycling the school is located on a strategic greenway with a combined pedestrian and cycle route off the highway. The bus route offers an alternative to using the car to access the school and associated uses.

The inclusion of a community hall as part of the Money Hill proposals is a direct response to an issue raised at public consultation. The potential community hall could provide a shared space for activities such as yoga, scouts/guides, antenatal classes and a community cinema club. It could be located either as part of the potential extra care facility, the primary school or the local centre are and, as a result, will benefit from being located near the bus route and has strong pedestrian linkages (see 5.3).

New public open spaces and sustainable drainage areas will be located along the watercourse corridor area, at Ivanhoe Meadows and in the Money Hill Park Fringe. All open spaces will be accessible by a series of formal and informal paths connecting pedestrian movement through the development and connect to the existing movement network (see 5.2.3).

Use	Amount	Guidance by
Residential	Up to 605 dwellings	Withdrawn LDF Core Strategy
Primary School Building	1,290sq.m	Leicestershire County Council
Community Hall	410sq.m	Principle emerged from public consultation
Local Centre Uses such as nursery and small commercial units	880sq.m	Public and stakeholder consultation
Open Space	33%	Withdrawn LDF Core Strategy and National Forest Company
Parking	110-120 parking spaces	Design led



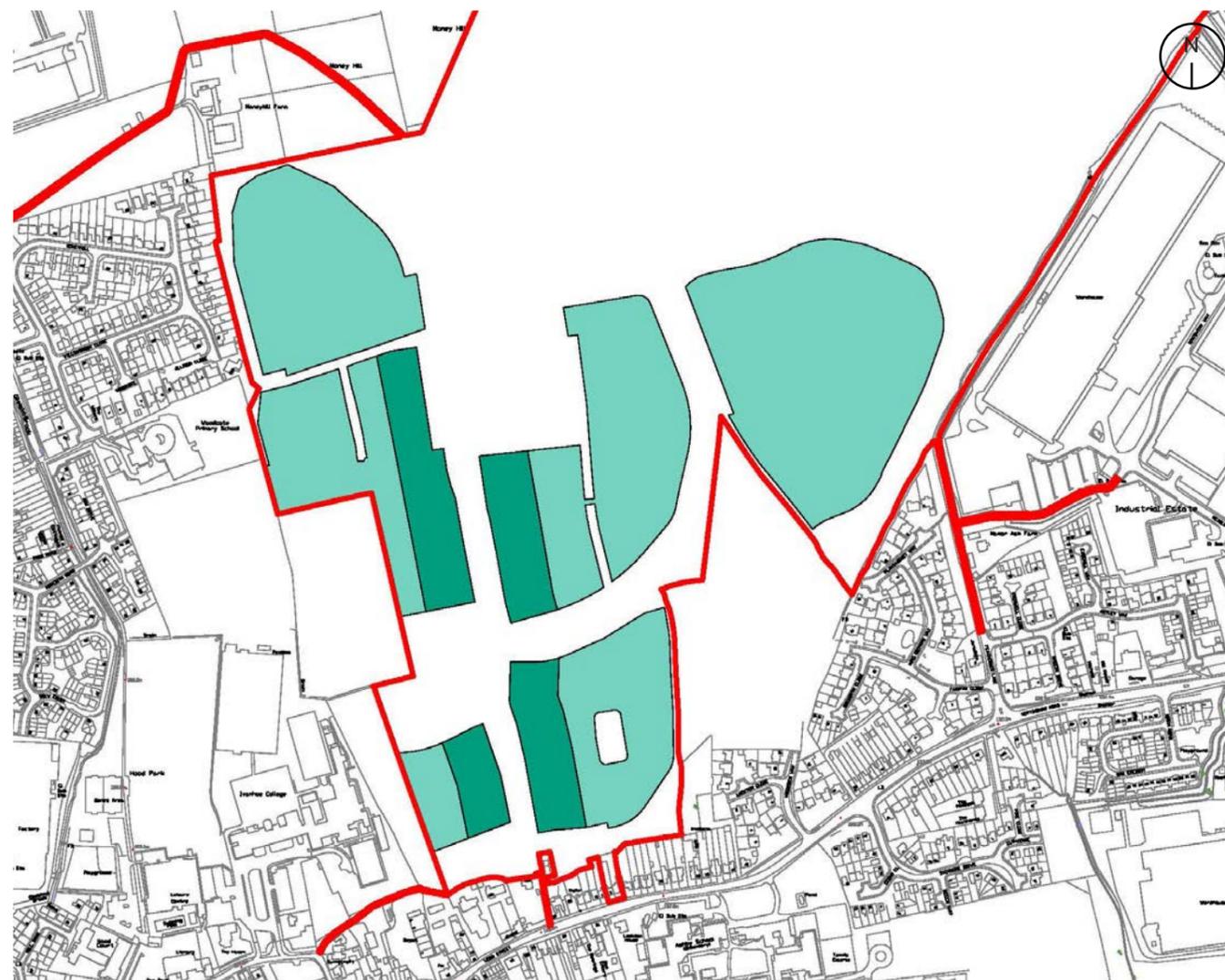
6.7 Amount and Density

Overall the 605 dwellings are distributed over 18.66Ha which produces an average density of 33 dwellings per hectare (D/Ha). This reflects a typical density if all areas that can potentially be used for houses. However, density may change if some of the facilities promoted are achieved. For instance an extra care use is likely to reach a higher density, so the general average density of the rest may have to reduce slightly to maintain the overall number of 605 dwellings.

The adjacent plan shows the proposed density of residential development across the site.

To limit the impact of development on the existing properties that neighbour the site, lower densities will occur where there the boundary is shared between new and existing development. This includes the rear boundaries of properties on Money Hill itself and properties off Wood Street. Lower densities will also occur along the watercourse to respond to the edge of the open space and the natural setting of the corridor.

- Potential for higher density
- Potential for lower density



Building Density Plan

6.8 Scale

The adjacent plan shows how building heights will be distributed across the site. The higher residential buildings will occur along the bus route to provide a sense of enclosure to the main spine road. Residential building heights will be the lowest on the higher ground and adjacent to open spaces. The school will be no more than 14.2m in height above the finished ground level to ensure that they are in keeping with the landform and surrounding residential development.

The table below illustrates the max and min parameters for scale and massing of all buildings by type that have been developed from the principles set out in 3.2.2. The height dimensions are in meters and are additional to finished the ground level (AOD). Most dimensions concern typical building with a mix of building widths and depths that allow for different life style choices. The topography of the site will also have a bearing on heights and care is needed to make buildings fit into the landscape setting.

- Ridge height not more than 11.7m above finished ground level. Accomodating maximum of 2.5 storey residential deep plan.
- Ridge height not more than 14.2m above finished ground level. Accomodating maximum of 3 storey residential inc. commercial ground floor.
- Ridge height not more than 14.2m above finished ground level. Accomodating maximum of either 3 storey residential or 2 storey other uses.

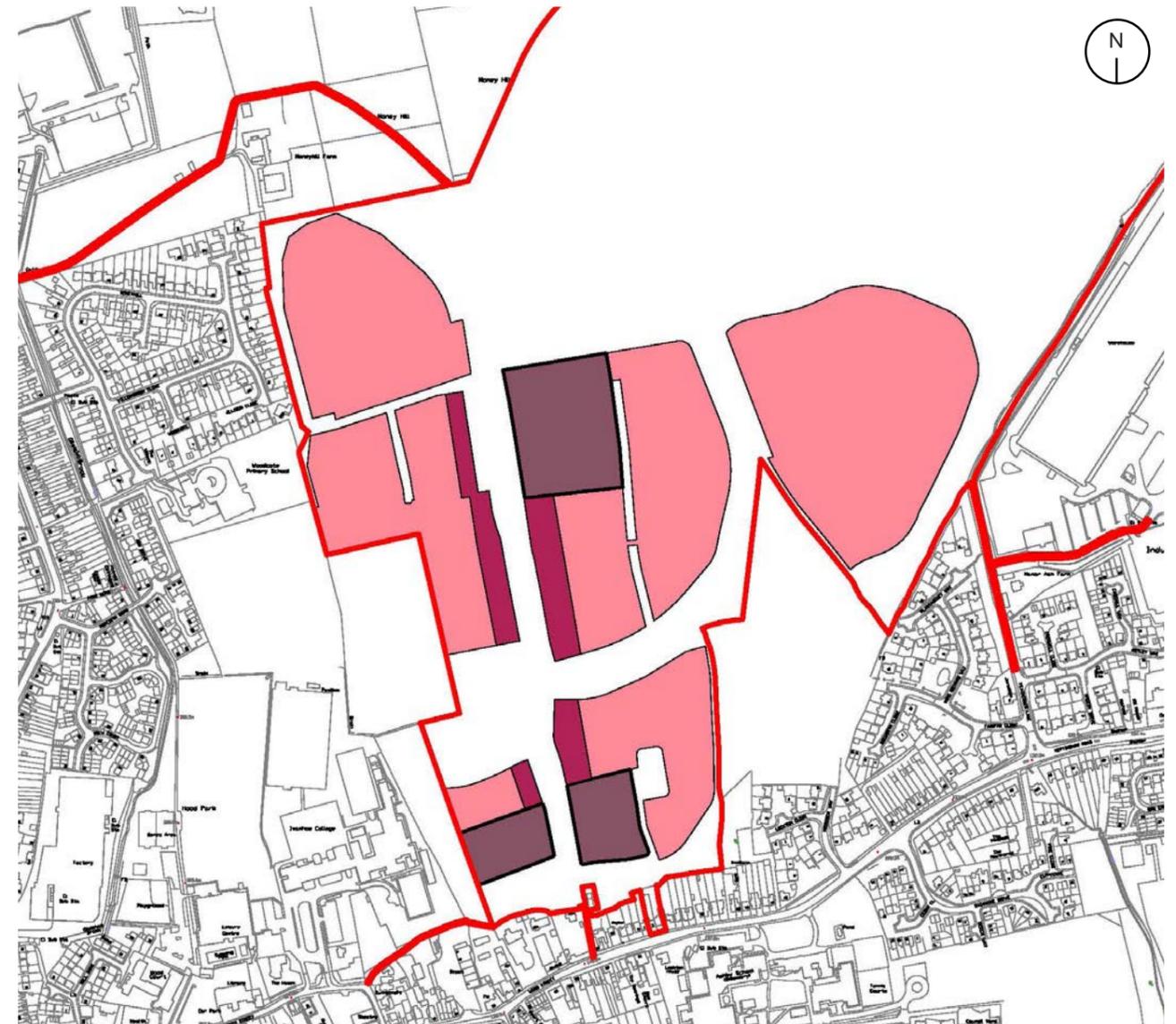
Existing levels to be modelled subject to drainage between 128.5m - 139.3m OAD within development area

Dimensions include height of ground floor slab of 0.15m
Residential floor to floor dimensions of 3m
Mixed use to floor dimensions of 4.0m

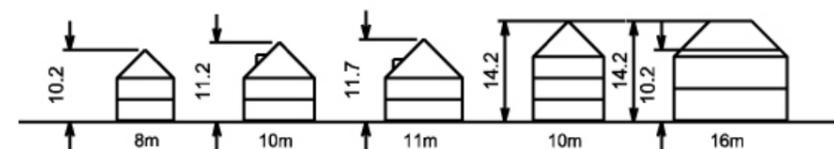
All dimensions are to ridge of building and above finished ground levels. Finished ground levels are generally existing levels. However, specific level increases of approximately 1m may occur on buildings close to the brook.

Building Type		Length (m)		Ridge Heights (m)		Storeys
		Distance across frontage	Depth from front to back	Highest point above AOD		
Typical terraces	Min.	13.5	5.5	8.5		1
	Max.	48	10	11		2.5
Semi-detached	Min.	10	5.5	8.5		1
	Max.	20	10	11		2.5
Detached	Min.	8	8	6		1
	Max.	12	11	11		2.5
Dwelling over Garage	Min.	10	6	5.5		2
	Max.	13	8	12		2.5
Garage	Min.	3	6	1.5		1
	Max.	12	6	6		1
Bin Stores / Ancillary	Min.	2	3	3		1
	Max.	5	5	3.5		1

The table shows the mix of building types that will be applied to the masterplan. This mix should be used as a guidance for the detailed arrangement of buildings within development blocks depending on location.



Building Scale Plan

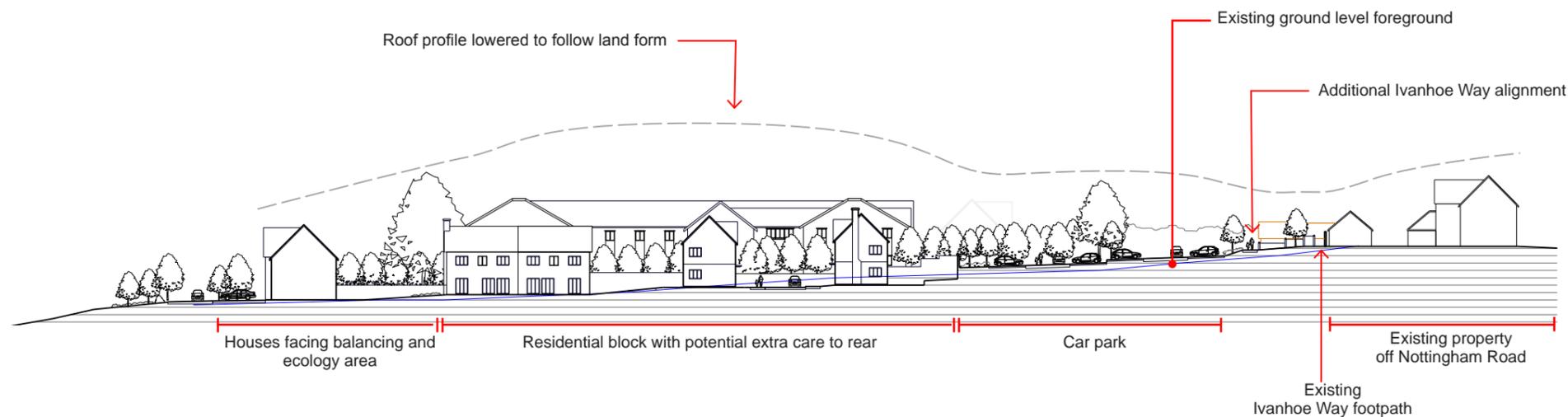


This diagram shows the maximum building heights for the proposed masterplan

The following cross sections demonstrate how the scale of buildings and landscape treatment of certain areas of the masterplan will work with the surrounding neighbourhoods and land form.

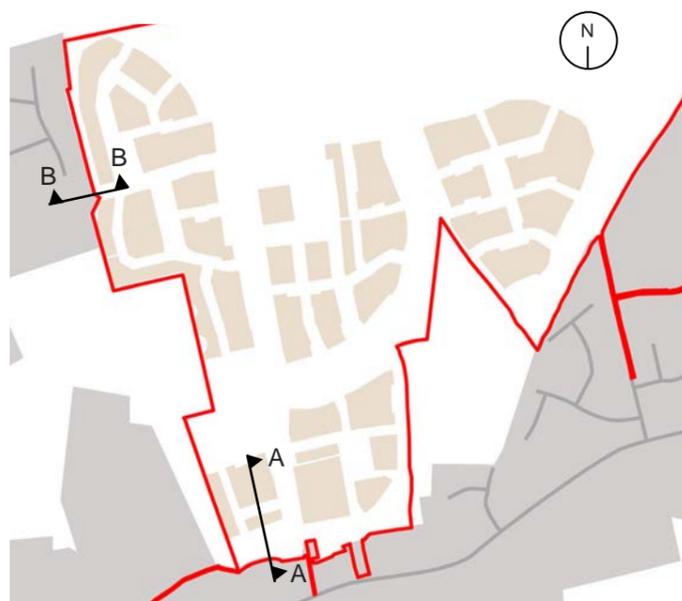
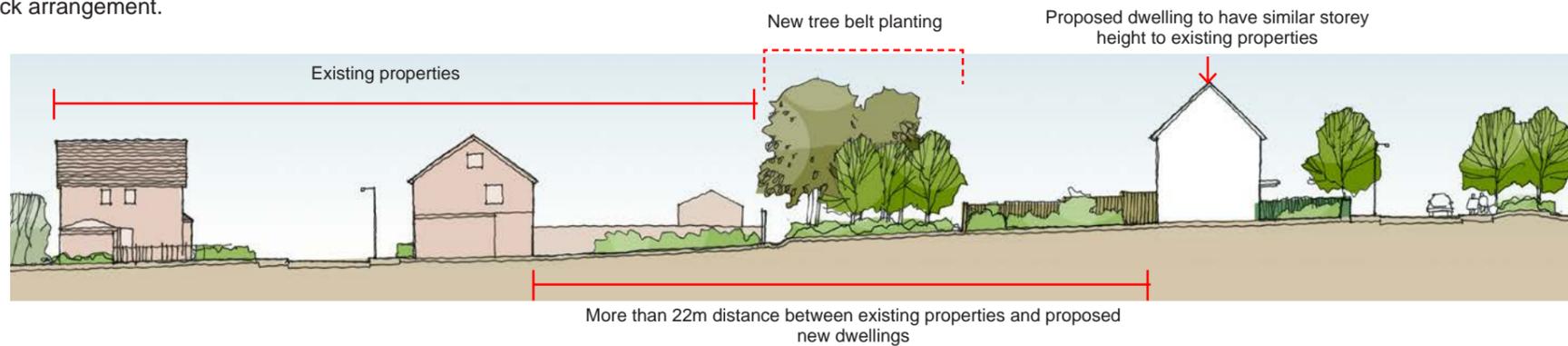
Section A A

This diagram shows how the scale and heights of buildings in the southern neighbourhood will respond to the changing ground level so to limit the impact on the existing properties off Wood Street and create a sense of place.



Section B B

This diagram shows how the proposed dwellings will be a similar scale to existing properties and creates a back to back block arrangement.



Cross Section Location Plan

7. IMPLEMENTATION

7.1 Phasing

It is important that a robust Phasing Strategy is put in place at an early stage of the Money Hill proposals in order to ensure a high quality development is achieved.

Although phasing will form part of a planning condition, the principles for phasing are:-

- 7.1.1 Access;
- 7.1.2 Land Preparation;
- 7.1.3 Servicing; and
- 7.1.4 Parcel Development.

7.1.1 Access

- Provide early stage construction access from the A511 to centre line of finished carriageway so no construction traffic passes along Nottingham Road;
- Ensure managed segregation between construction routes and finished highways to reduce construction impacts on newly settled residents;
- Provide finished road surface and landscaped features to the main approach road from the A511 and seek early adoption;
- Provide pedestrian / cycleway link to Smisby Road to allow new residents access to existing school; and
- The managed decommission of haul road within agricultural land.



7.1.2 Land Preparation

- Implement green infrastructure at an early stage to protect important landscape features of the Masterplan through the construction process;
- Provide early planting of Ivanhoe Way Meadows so that existing Nottingham Road residents retain landscape amenity at an early stage of construction; and
- Provide early land modelling to allow wetland ecology areas to become established.



7.1.3 Servicing

- Provide utilities, drainage infrastructure and connection to existing utility networks ahead of parcel release for development; and
- Provide internal street construction sufficient to access development parcels in a staged manor, particularly to ensure that planning conditions for health, education, social and medical use parcels are adhered to.

7.1.4 Parcel Development

- Build out residential early stage to meet NWLDC 5 year housing needs;
- Potential early release of land for build out of the community uses; and
- Ensure the primary school is complete on the 300th dwelling occupation.

